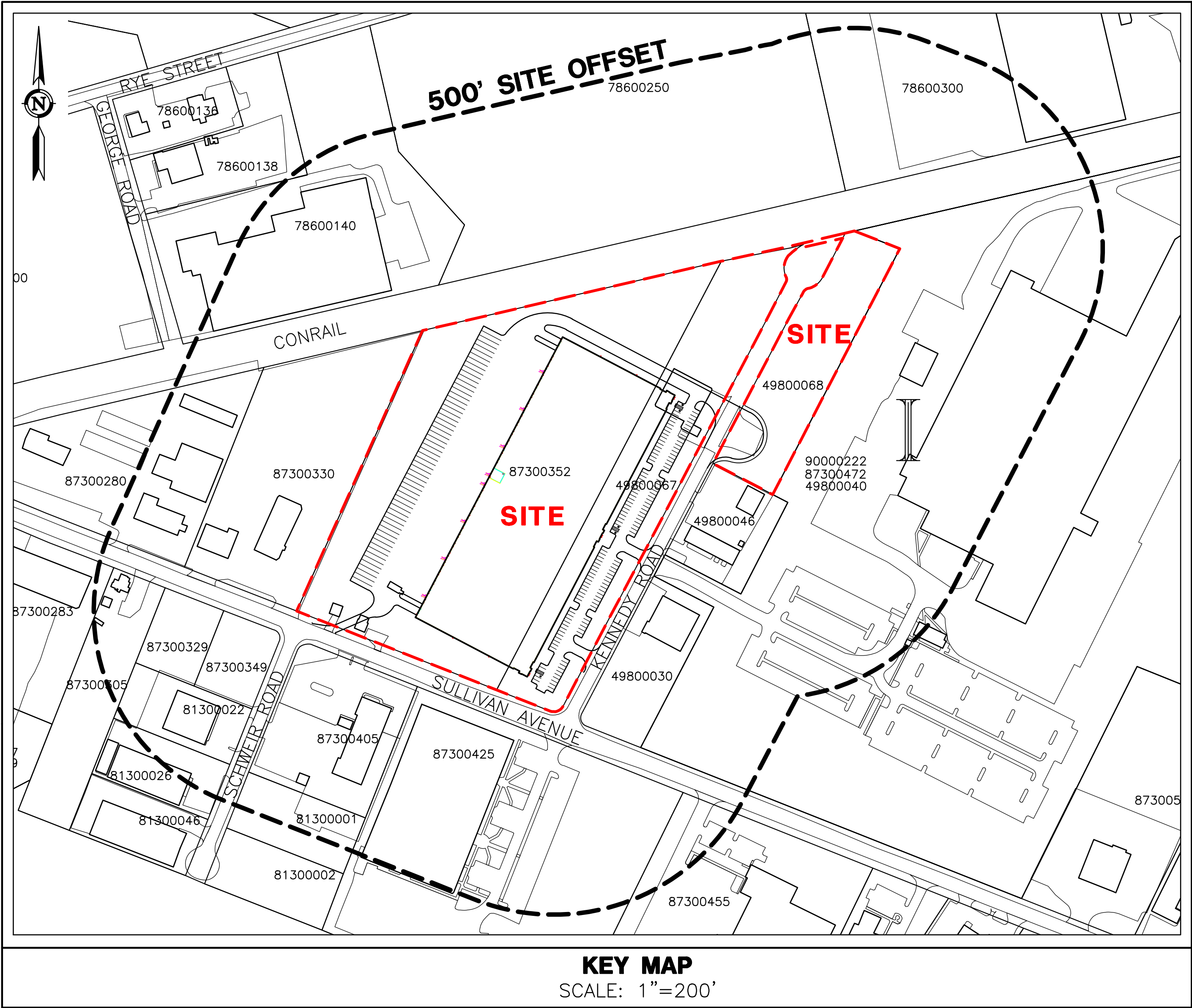
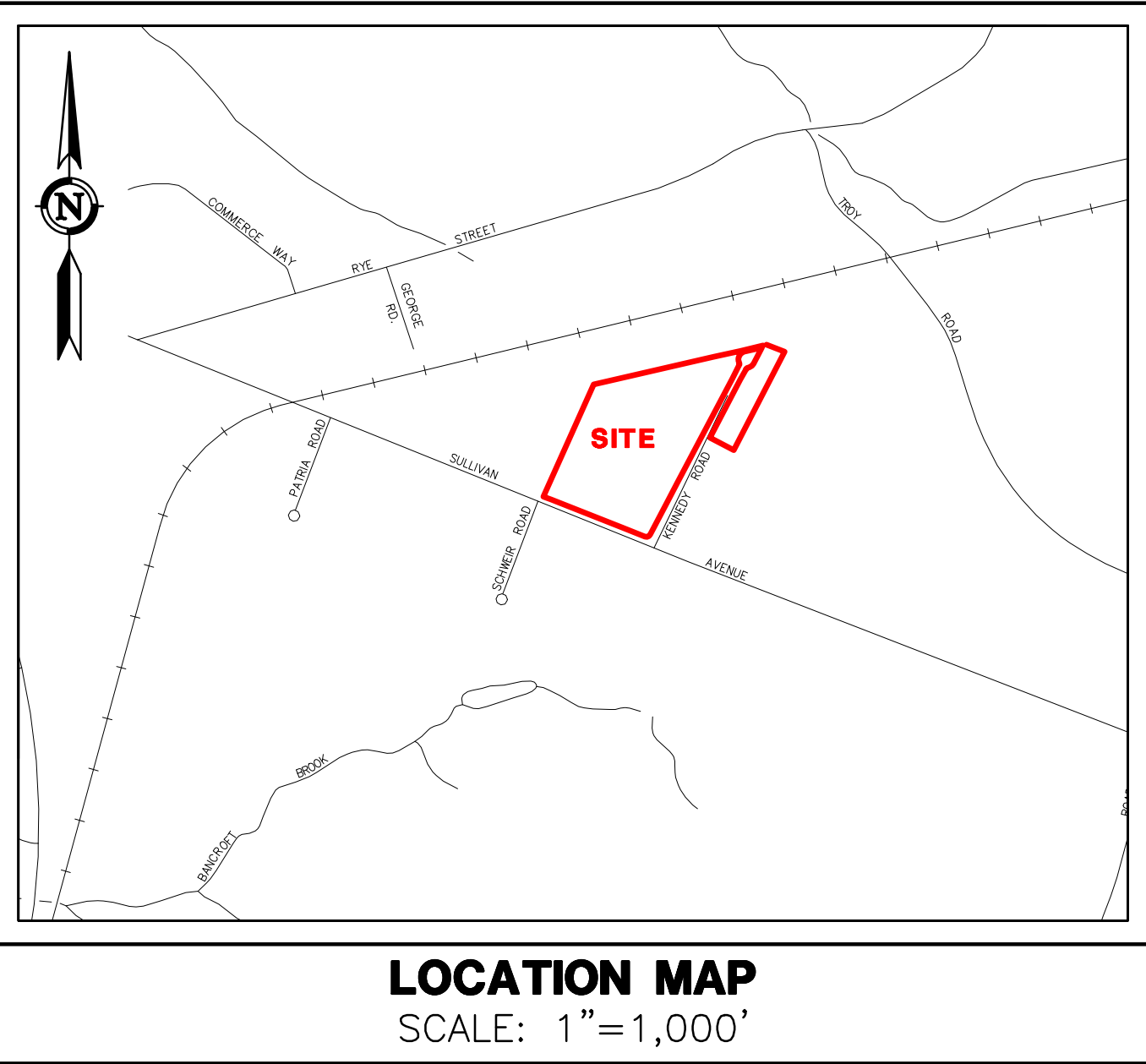


67 KENNEDY ROAD WAREHOUSE & DISTRIBUTION CENTER

SITE PLAN

352 SULLIVAN AVENUE, 67 & 68 KENNEDY ROAD ~ SOUTH WINDSOR ~ CT
GIS NOs. 87300352, 49800067, & 49800068

N/F 500' ABUTTERS		
STREET ADDRESS	OWNER	PARCEL ID
30 KENNEDY ROAD	WOODCOCK JOHN J JR TR	49800030
40 KENNEDY ROAD	456 SULLIVAN AVE LLC	49800040
46 KENNEDY ROAD	LAWSON'S PROPERTIES LLC	49800046
138 RYE STREET	SHEPARD POLA INC	78600138
140 RYE STREET	SHEPARD POLA INC	78600140
250 RYE STREET	ALTATWO REALTY COMPANY LLC	78600250
300 RYE STREET	ALTA REALTY COMPANY LLC	78600300
1 SCHWEIR ROAD	KUHNS FAMILY PROPERTIES LLC	81300001
22 SCHWEIR ROAD	WOLF & GUERRA LLC	81300022
26 SCHWEIR ROAD	MISCELLANEOUS STEEL & RAIL LLC	81300026
280 SULLIVAN AVENUE	280 SULLIVAN AVENUE LLC	87300280
283 SULLIVAN AVENUE	DELLA N LLC/LEGACY VENTURES LLC	87300283
305 SULLIVAN AVENUE	WOLF HELMAR	87300305
329 SULLIVAN AVENUE	WOLF & GUERRA LLC	87300329
330 SULLIVAN AVENUE	JKLN LLC	87300330
349 SULLIVAN AVENUE	WOLF & GUERRA LLC	87300349
405 SULLIVAN AVENUE	KUHNS FAMILY PROPERTIES LLC	87300405
425 SULLIVAN AVENUE	CONDOR PROPERTIES LLC	87300425
455 SULLIVAN AVENUE	PARKSITE INC	87300455
472 SULLIVAN AVENUE	CURRENT RESIDENT	87300472
222 TROY ROAD	CURRENT RESIDENT	90000222



SHEET INDEX		
C-T1	COVER SHEET	1 of 24
C-OS1	OVERALL SITE PLAN	2 of 24
C-SP1 - C-SP2	SITE PLAN	3-4 of 24
C-GD1 - C-GD2	GRADING PLAN	5-6 of 24
C-DR1 - C-DR2	DRAINAGE PLAN	7-8 of 24
C-UT1 - C-UT2	UTILITY PLAN	9-10 of 24
C-ES1 - C-ES2	EROSION & SEDIMENTATION PLAN	11-12 of 24
C-ES3	EROSION & SEDIMENTATION NOTES & DETAILS	13 of 24
C-LS1 - C-LS2	LANDSCAPE PLAN	14-15 of 24
C-LS3	LANDSCAPE NOTES & DETAILS	16 of 24
C-LT1 - C-LT2	LIGHTING PLAN	17-18 of 24
C-D1	NOTES, DETAILS, & LEGEND	19 of 24
C-D2 - C-D6	DETAILS	20-24 of 24
V-1 TO V-2	PROPERTY & TOPOGRAPHIC SURVEY	1-2 of 2
PROPOSED OFFICE/WAREHOUSE FLOOR PLAN & ELEVATIONS		

**PRELIMINARY
NOT FOR CONSTRUCTION**
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Fax: 860-291-8757
www.designprofessionalsinc.com

TRAFFIC ENGINEER:

Langan
555 Long Wharf Drive
New Haven, CT 06511-6107
203-563-5771
www.langan.com

PROPERTY OWNER:

FOUR WOODS LLC
C/O JOHN J WOODCOCK III
89 SADDLEBACK DRIVE
SOUTH WINDSOR, CT 06074
APPLICANT:
SCANNELL PROPERTIES #644, LLC
294 GROVE LANE EAST
SUITE 140
WAYZATA, MN 55391
(763) 331-8851

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P.O. BOX 1167
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763-331-8851 T

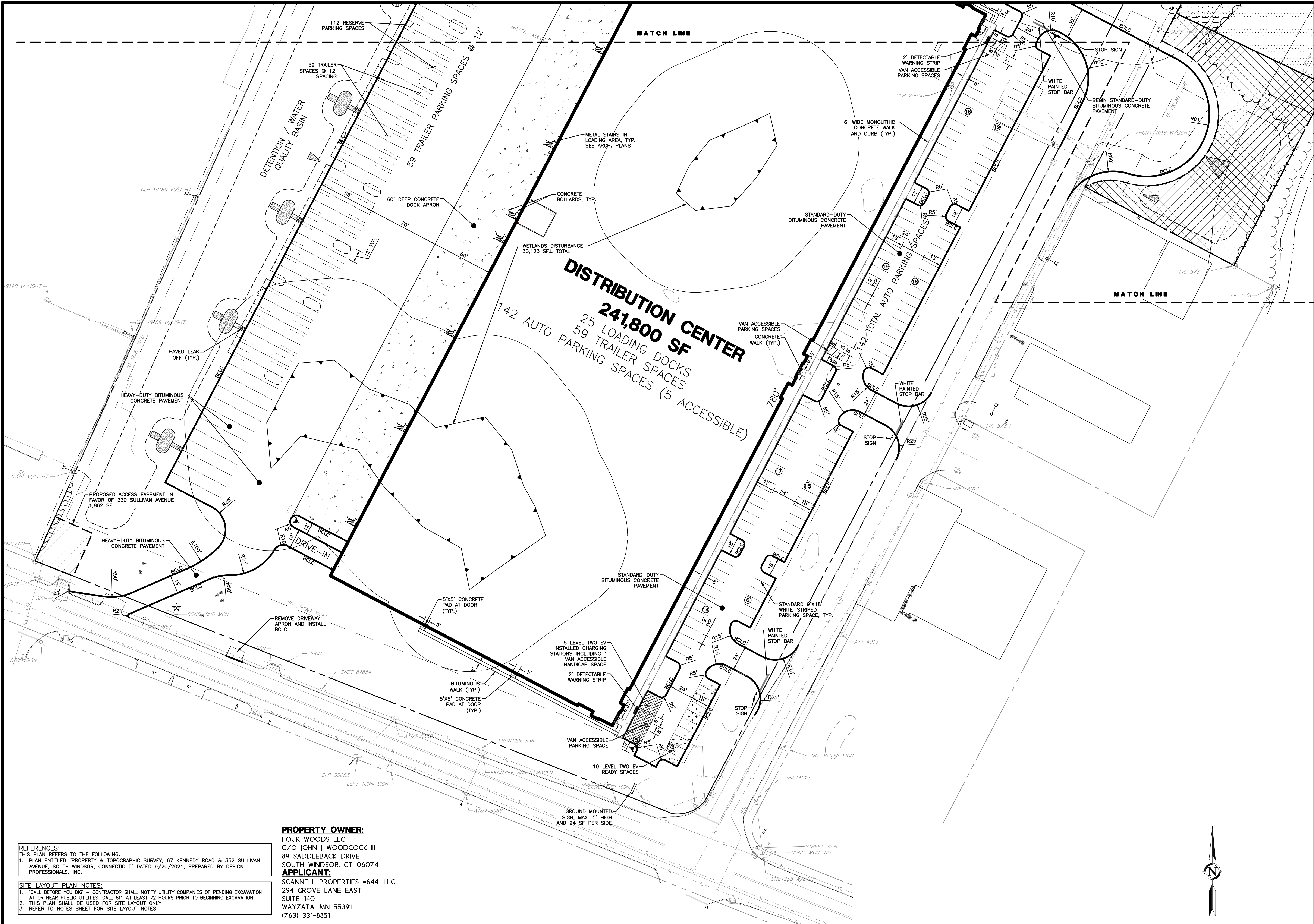
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4670
DATE:
3/30/2022
DESIGNED BY:
DHI/BPW
CHECKED BY:
DHI/BPW

**67 KENNEDY ROAD
WAREHOUSE &
DISTRIBUTION CENTER**
352 SULLIVAN AVENUE, 67 & 68 KENNEDY ROAD
SOUTH WINDSOR, CONNECTICUT 06074
GIS NOs: 87300352, 49800067, & 49800068

NO.	DATE	REVISIONS
1	4/7/22	EGS PLAN UPDATES
2	4/20/22	P2C SUBMISSION

TITLE

SHEET
C-T1
SHEET 1 OF 24



REFERENCES:
THIS PLAN REFERS TO THE FOLLOWING:
1. PLAN ENTITLED "PROPERTY & TOPOGRAPHIC SURVEY, 67 KENNEDY ROAD & 352 SULLIVAN AVENUE, SOUTH WINDSOR, CONNECTICUT" DATED 9/20/2021, PREPARED BY DESIGN PROFESSIONALS, INC.

SITE LAYOUT PLAN NOTES:
1. *CALL BEFORE YOU DIG* - CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF PENDING EXCAVATION AT OR NEAR PUBLIC UTILITIES. CALL 811 AT LEAST 72 HOURS PRIOR TO BEGINNING EXCAVATION.
2. THIS PLAN SHALL BE USED FOR SITE LAYOUT ONLY
3. REFER TO NOTES SHEET FOR SITE LAYOUT NOTES

PROPERTY OWNER:
FOUR WOODS LLC
C/O JOHN J WOODCOCK III
89 SADDLEBACK DRIVE
SOUTH WINDSOR, CT 06074

APPLICANT:
SCANNELL PROPERTIES #644, LLC
294 GROVE LANE EAST
SUITE 140
WAYZATA, MN 55391
(763) 331-8851

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860-291-8737 - F
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294 Grove Lane East
Suite 140
Wayzata, MN 55391
763-331-8851 T

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DATE: 3/30/2022
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DRAWN BY: DHJ/BPW
CHECKED BY: DHJ/BPW
CONVERTED BY: BPW

**67 KENNEDY ROAD
WAREHOUSE &
DISTRIBUTION CENTER**
352 SULLIVAN AVENUE, 67 & 68 KENNEDY ROAD
SOUTH WINDSOR, CONNECTICUT 06074
GIS NOS: 87300352, 49800067, & 49800068

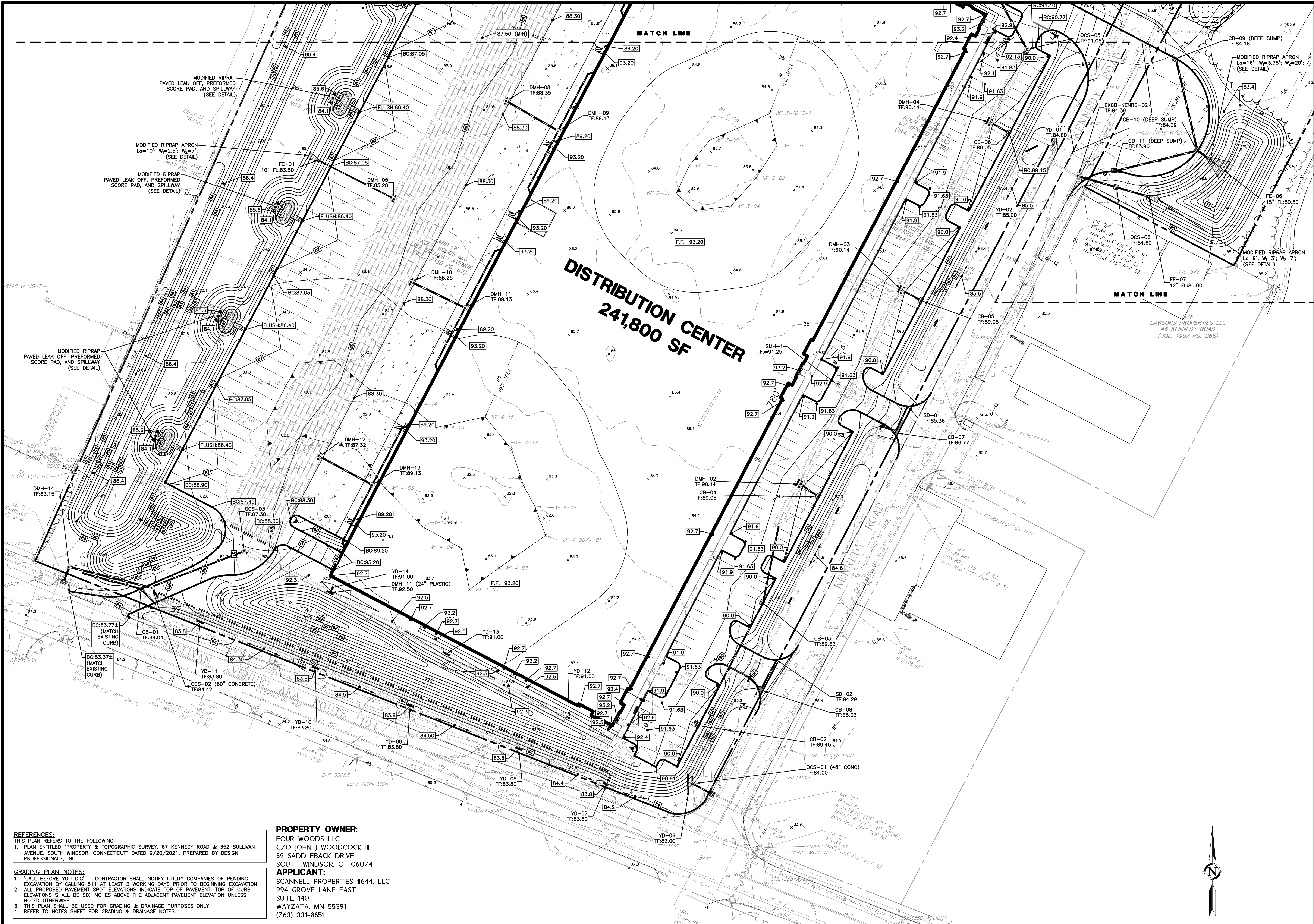
REVISIONS

NO.	DATE	BY
1	4/7/22	DHJ
2	4/20/22	DHJ

SITE PLAN

SCALE: 0' 20' 40' 80'
T = 40'

C-SP1
SHEET 3 OF 24



REFERENCES:
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GRADING PLAN NOTES:
1. "CALL BEFORE YOU DIG" - CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF PENDING EXCAVATION BY CALLING 811 AT LEAST 3 WORKING DAYS PRIOR TO BEGINNING EXCAVATION.
2. ALL PROPOSED PAVEMENT SPOT ELEVATIONS INDICATE TOP OF PAVEMENT. TOP OF CURB ELEVATIONS SHALL BE SIX INCHES ABOVE THE ADJACENT PAVEMENT ELEVATION UNLESS NOTED OTHERWISE.
3. THIS PLAN SHALL BE USED FOR GRADING & DRAINAGE PURPOSES ONLY
4. REFER TO NOTES SHEET FOR GRADING & DRAINAGE NOTES

PROPERTY OWNER:
FOUR WOODS LLC
C/O JOHN J WOODCOCK III
89 SADDLEBACK DRIVE
SOUTH WINDSOR, CT 06074

APPLICANT:
SCANNELL PROPERTIES #644, LLC
294 GROVE LANE EAST
SUITE 140
WAYZATA, MN 55391
(763) 331-8851

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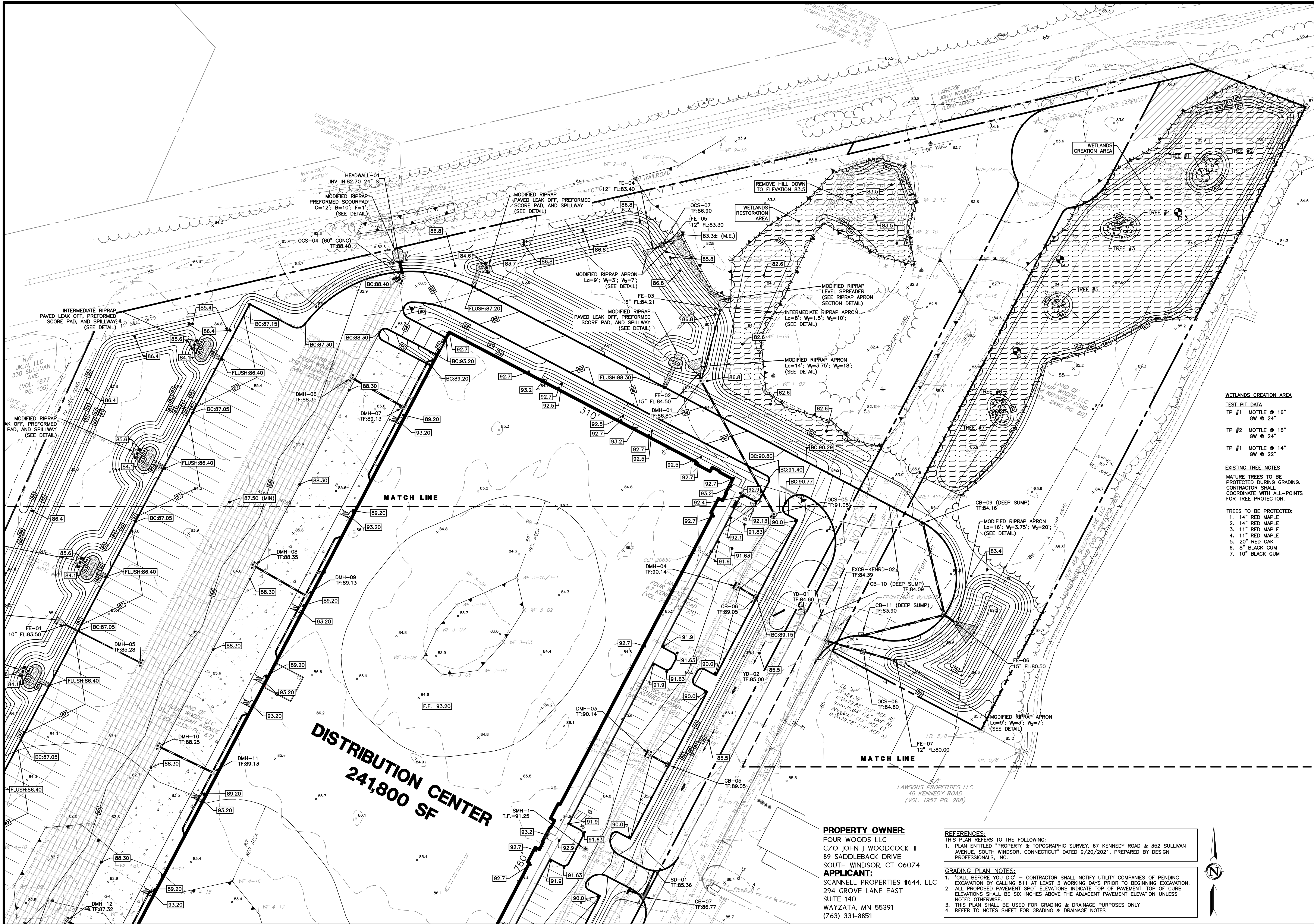
NO.	DATE	BY
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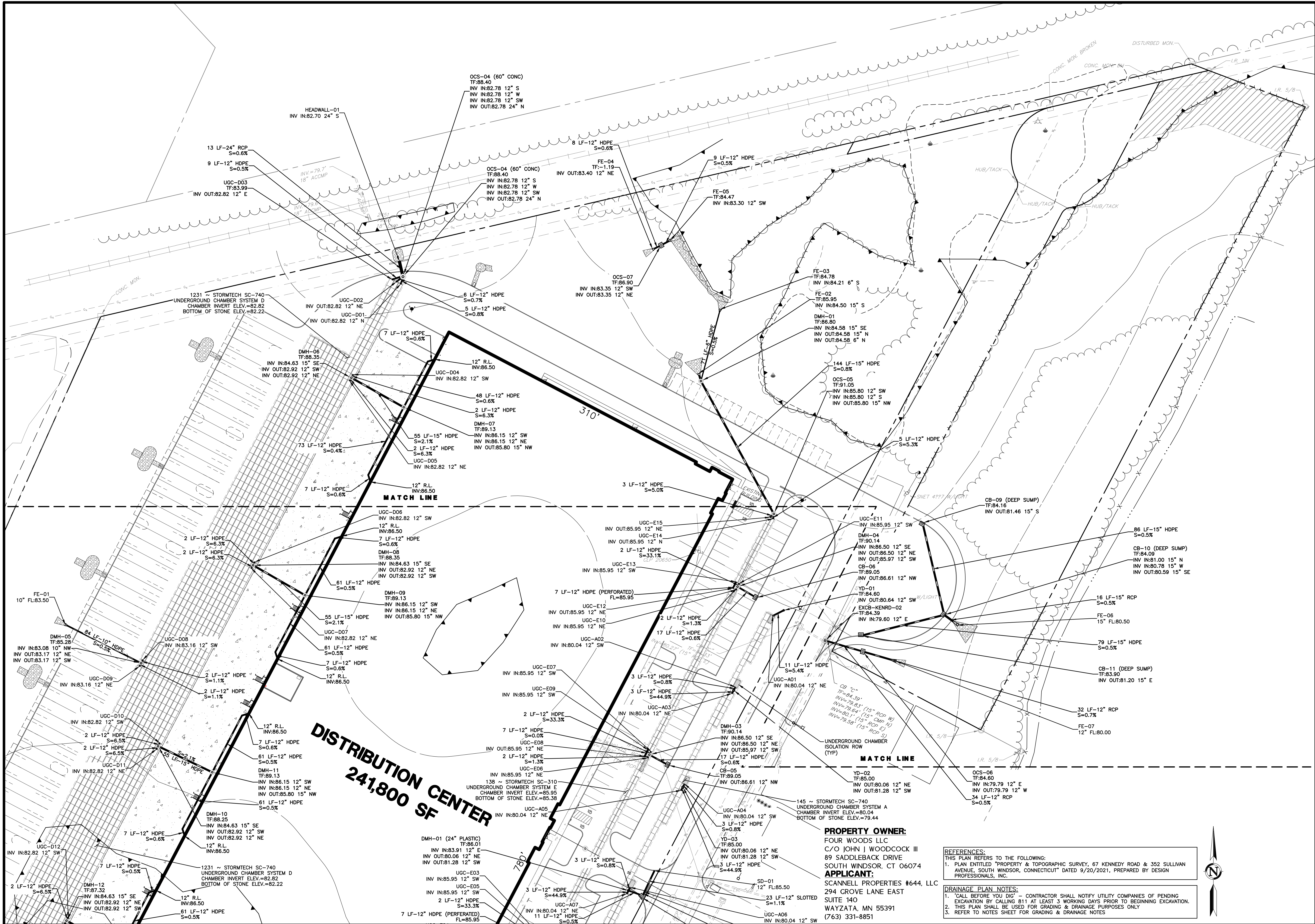
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GRADING PLAN

SCALE: 0 20' 40'
1" = 40'

SHEET
C-GD1
SHEET 5 OF 24





REFERENCES:
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1. PLAN ENTITLED "PROPERTY & TOPOGRAPHIC SURVEY, 67 KENNEDY ROAD & 352 SULLIVAN AVENUE, SOUTH WINDSOR, CONNECTICUT 06074", DATED 9/20/2021, PREPARED BY DESIGN PROFESSIONALS, INC.

DRAINAGE PLAN NOTES:
1. "CALL BEFORE YOU DIG" - CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF PENDING EXCAVATION BY CALLING 811 AT LEAST 3 WORKING DAYS PRIOR TO BEGINNING EXCAVATION.
2. THIS PLAN SHALL BE USED FOR GRADING & DRAINAGE PURPOSES ONLY.
3. REFER TO NOTES SHEET FOR GRADING & DRAINAGE NOTES

REVISIONS

NO.	DATE	BY	REVISIONS
1	4/7/22	DHJ	EAS PLAN UPDATES
2	4/20/22	DHJ	PIC SUBMISSION

DRAINAGE PLAN

SHEET 8 OF 24

PROPERTY OWNER:
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C/O JOHN J WOODCOCK III
89 SADDLEBACK DRIVE
SOUTH WINDSOR, CT 06074

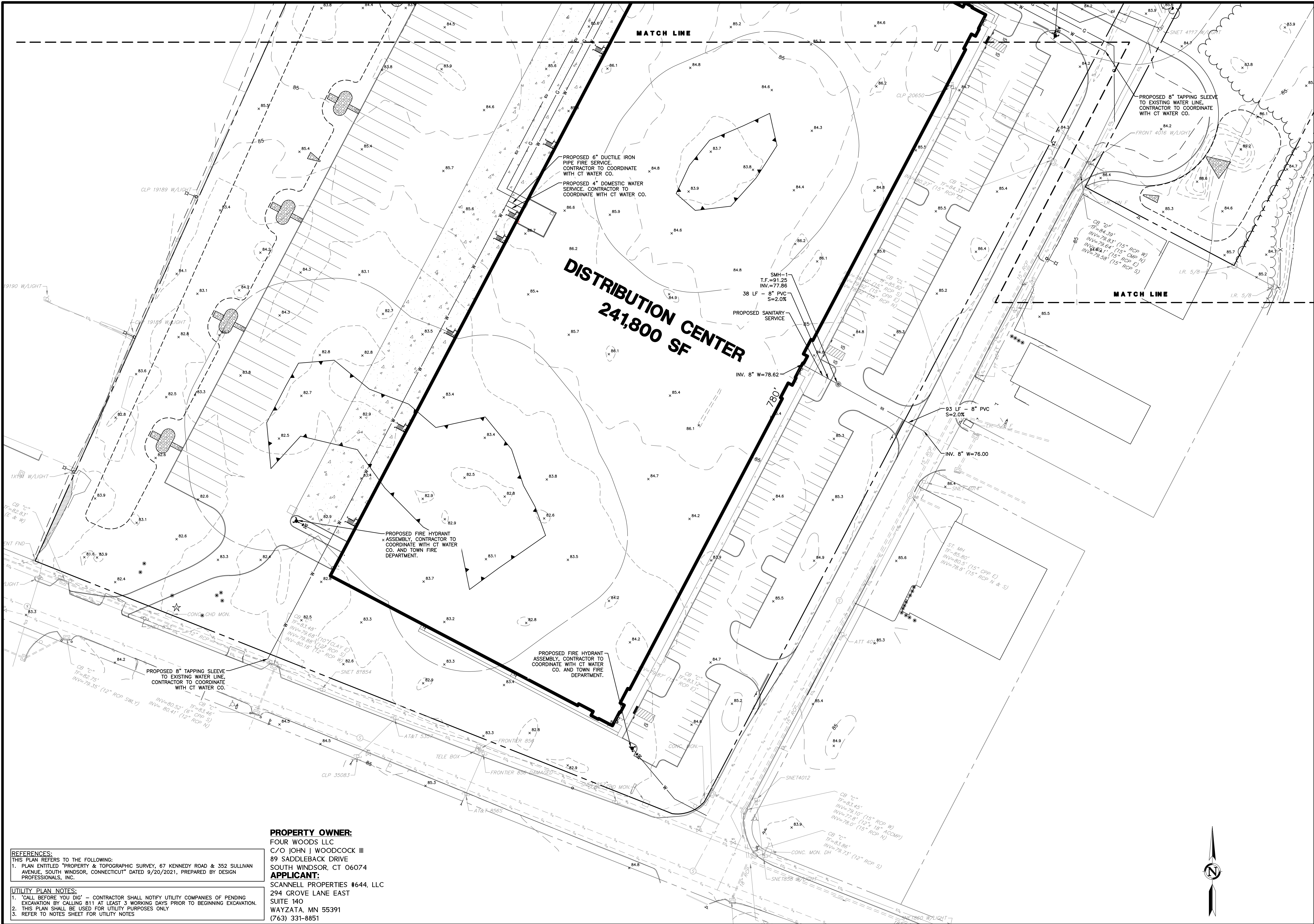
APPLICANT:
SCANNELL PROPERTIES #644, LLC
294 GROVE LANE EAST
SUITE 140
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DATE: [blank]



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UTILITY PLAN NOTES:
1. "CALL BEFORE YOU DIG" - CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF PENDING EXCAVATION BY CALLING 811 AT LEAST 3 WORKING DAYS PRIOR TO BEGINNING EXCAVATION.
2. THIS PLAN SHALL BE USED FOR UTILITY PURPOSES ONLY
3. REFER TO NOTES SHEET FOR UTILITY NOTES

PROPERTY OWNER:
FOUR WOODS LLC
C/O JOHN J WOODCOCK III
89 SADDLEBACK DRIVE
SOUTH WINDSOR, CT 06074

APPLICANT:
SCANNELL PROPERTIES #644, LLC
294 GROVE LANE EAST
SUITE 140
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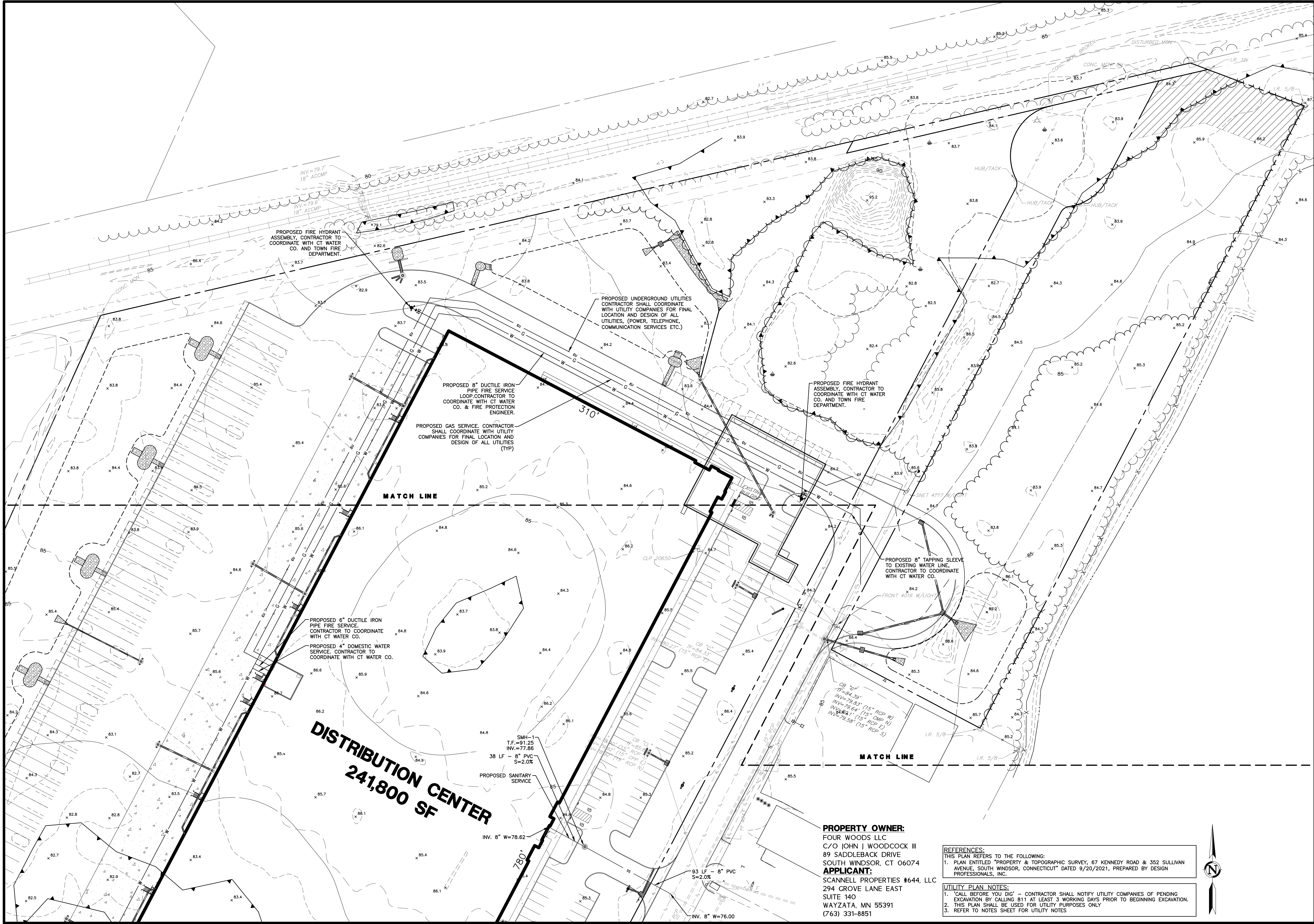
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		DRAWN BY DHL/BPW	CHECKED BY DHL/BPW
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UTILITY PLAN

SCALE: 0' 20' 40' 80'
T = 40'

SHEET
C-UT1
SHEET 5 OF 24



PROPERTY OWNER:
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C/O JOHN J WOODCOCK III
89 SADDLEBACK DRIVE
SOUTH WINDSOR, CT 06074
APPLICANT:
SCANNELL PROPERTIES #644, LLC
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UTILITY PLAN NOTES:
1. "CALL BEFORE YOU DIG" - CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF PENDING EXCAVATION BY CALLING 811 AT LEAST 3 WORKING DAYS PRIOR TO BEGINNING EXCAVATION.
2. THIS PLAN SHALL BE USED FOR UTILITY PURPOSES ONLY
3. REFER TO NOTES SHEET FOR UTILITY NOTES

UTILITY PLAN

REVISIONS

NO.	DATE	BY	REVISIONS
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SCALE: 0 20' 40' 80'

SHEET 10 OF 24

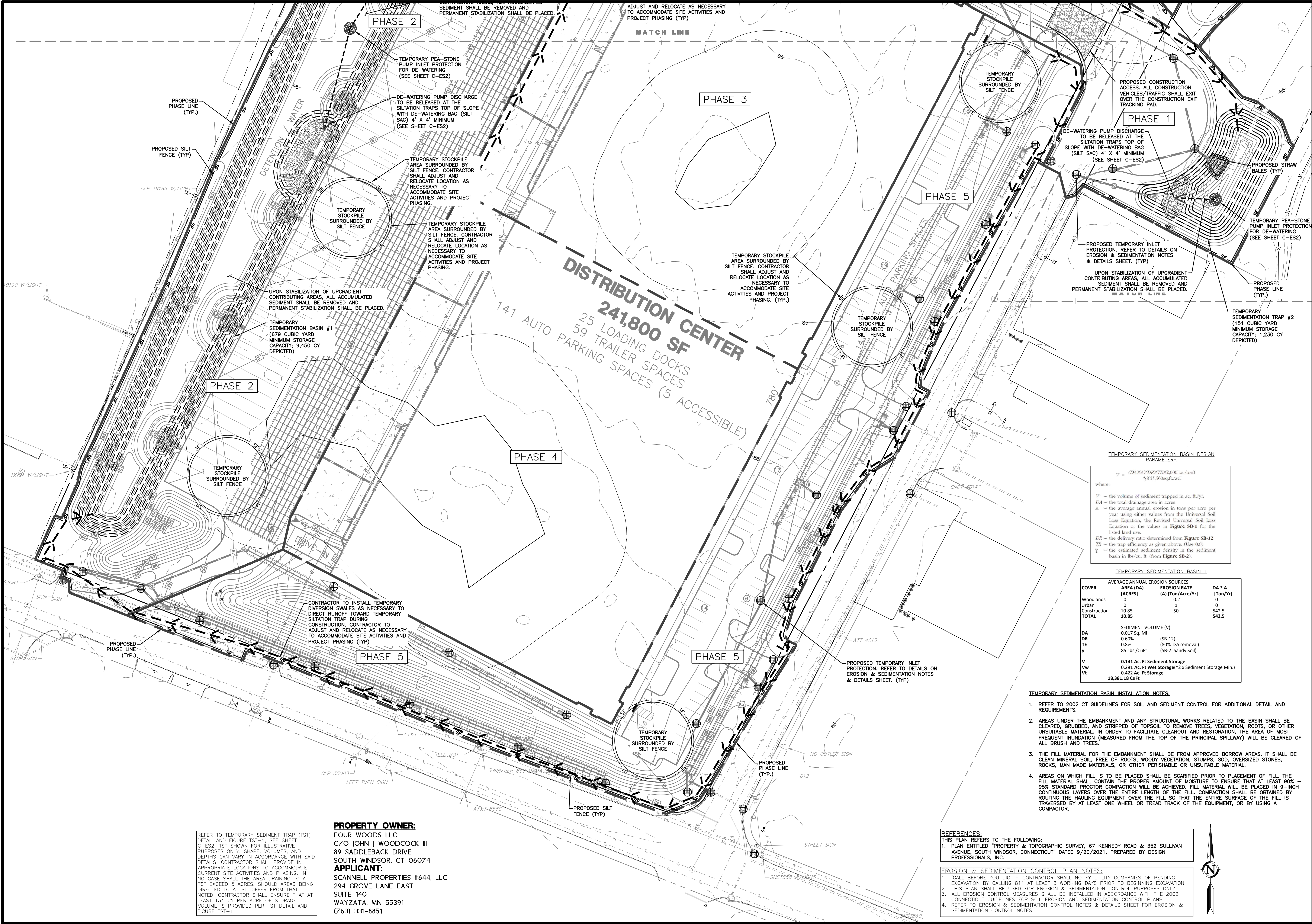
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GIS NOS: 87300352, 49800067, & 49800068

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PROJECT NO.
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DESIGNED BY
DHJ/BPW
CHECKED BY
CJW/BPW

DESIGN PROFESSIONALS, INC.
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P.O. BOX 167
SOUTH WINDSOR, CT 06074
860-291-8727 - F
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REFER TO TEMPORARY SEDIMENT TRAP (TST) DETAIL AND FIGURE TST-1, SEE SHEET C-ES2. TST SHOWN FOR ILLUSTRATIVE PURPOSES ONLY. SHAPE, VOLUMES, AND DEPTHS CAN VARY IN ACCORDANCE WITH SAID DETAILS. CONTRACTOR SHALL PROVIDE IN APPROPRIATE LOCATIONS TO ACCOMMODATE CURRENT SITE ACTIVITIES AND PHASING. IN NO CASE SHALL THE AREA DRAINING TO A TST EXCEED 5 ACRES. SHOULD AREAS BEING DIRECTED TO A TST DIFFER FROM THAT NOTED, CONTRACTOR SHALL ENSURE THAT AT LEAST 134 CY PER ACRE OF STORAGE VOLUME IS PROVIDED PER TST DETAIL AND FIGURE TST-1.

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SOUTH WINDSOR, CT 06074
APPLICANT:
SCANNELL PROPERTIES #644, LLC
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(763) 331-8851

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EROSION & SEDIMENTATION CONTROL PLAN NOTES:
1. "CALL BEFORE YOU DIG" - CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF PENDING EXCAVATION BY CALLING 811 AT LEAST 3 WORKING DAYS PRIOR TO BEGINNING EXCAVATION.
2. THIS PLAN SHALL BE USED FOR EROSION & SEDIMENTATION CONTROL PURPOSES ONLY.
3. ALL EROSION CONTROL MEASURES SHALL BE INSTALLED IN ACCORDANCE WITH THE 2002 CONNECTICUT GUIDELINES FOR SOIL EROSION AND SEDIMENTATION CONTROL PLANS.
4. REFER TO EROSION & SEDIMENTATION CONTROL NOTES & DETAILS SHEET FOR EROSION & SEDIMENTATION CONTROL NOTES.

TEMPORARY SEDIMENTATION BASIN DESIGN PARAMETERS

$$V = (DA)(A)(DR)(TE)(2,000 \text{ lbs./ton})$$

where:
 V = the volume of sediment trapped in ac. ft./yr.
 DA = the total drainage area in acres
 A = the average annual erosion in tons per acre per year using either values from the Universal Soil Loss Equation, the Revised Universal Soil Loss Equation or the values in Figure SB-1 for the listed land use.
 DR = the delivery ratio determined from Figure SB-12.
 TE = the trap efficiency as given above. (Use 0.8)
 γ = the estimated sediment density in the sediment basin in lbs/cu. ft. (from Figure SB-2).

TEMPORARY SEDIMENTATION BASIN #1			
AVERAGE ANNUAL EROSION SOURCES			
COVER	AREA (DA) [ACRES]	EROSION RATE (A) [Ton/Acre/Yr]	DA * A [Ton/Yr]
Woodlands	0	0.2	0
Urban	0	1	0
Construction	10.85	50	542.5
TOTAL	10.85		542.5
SEDIMENT VOLUME (V)			
DA	0.017 Sq. Mi		
DR	0.60% (SB-12)		
TE	0.8% (80% TSS removal)		
V	85 Lbs./CuFt (SB-2: Sandy Soil)		
V	0.141 Ac. Ft Sediment Storage		
Vw	0.281 Ac. Ft Wet Storage (2 x Sediment Storage Min.)		
Vt	0.422 Ac. Ft Storage		
	18,381.18 CuFt		

- TEMPORARY SEDIMENTATION BASIN INSTALLATION NOTES:**
- REFER TO 2002 CT GUIDELINES FOR SOIL AND SEDIMENT CONTROL FOR ADDITIONAL DETAIL AND REQUIREMENTS.
 - AREAS UNDER THE EMBANKMENT AND ANY STRUCTURAL WORKS RELATED TO THE BASIN SHALL BE CLEARED, GRUBBED, AND STRIPPED OF TOPSOIL TO REMOVE TREES, VEGETATION, ROOTS, OR OTHER UNSUITABLE MATERIAL. IN ORDER TO FACILITATE CLEANOUT AND RESTORATION, THE AREA OF MOST FREQUENT INUNDATION (MEASURED FROM THE TOP OF THE PRINCIPAL SPILLWAY) WILL BE CLEARED OF ALL BRUSH AND TREES.
 - THE FILL MATERIAL FOR THE EMBANKMENT SHALL BE FROM APPROVED BORROW AREAS. IT SHALL BE CLEAN MINERAL SOIL, FREE OF ROOTS, WOODY VEGETATION, STUMPS, SOD, OVERSIZED STONES, ROCKS, MAN MADE MATERIALS, OR OTHER PERISHABLE OR UNSUITABLE MATERIAL.
 - AREAS ON WHICH FILL IS TO BE PLACED SHALL BE SCARIFIED PRIOR TO PLACEMENT OF FILL. THE FILL MATERIAL SHALL CONTAIN THE PROPER AMOUNT OF MOISTURE TO ENSURE THAT AT LEAST 90% - 95% STANDARD PROCTOR COMPACTION WILL BE ACHIEVED. FILL MATERIAL WILL BE PLACED IN 9-INCH CONTINUOUS LAYERS OVER THE ENTIRE LENGTH OF THE FILL. COMPACTION SHALL BE OBTAINED BY ROUTING THE HAULING EQUIPMENT OVER THE FILL SO THAT THE ENTIRE SURFACE OF THE FILL IS TRAVERSED BY AT LEAST ONE WHEEL OR TREAD TRACK OF THE EQUIPMENT, OR BY USING A COMPACTOR.

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P.O. BOX 167
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294 Grove Lane East
Suite 140
Wayzata, MN 55391
763-331-8851 T

PROJECT NO.:
4670

DATE:
3/30/2022

DESIGN BY:
DJI/BPW

REVIEW BY:
CONCURRED BY:
BPW

**67 KENNEDY ROAD
WAREHOUSE &
DISTRIBUTION CENTER**

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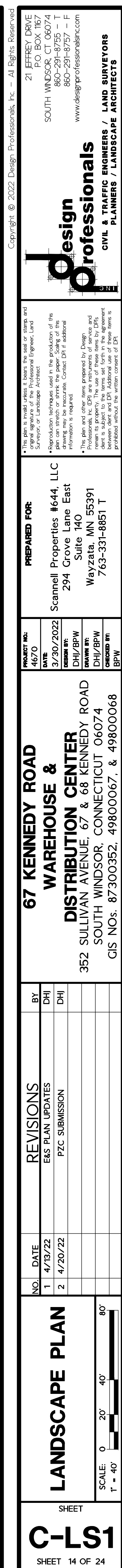
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1	4/19/22	DJI
2	4/20/22	DJI

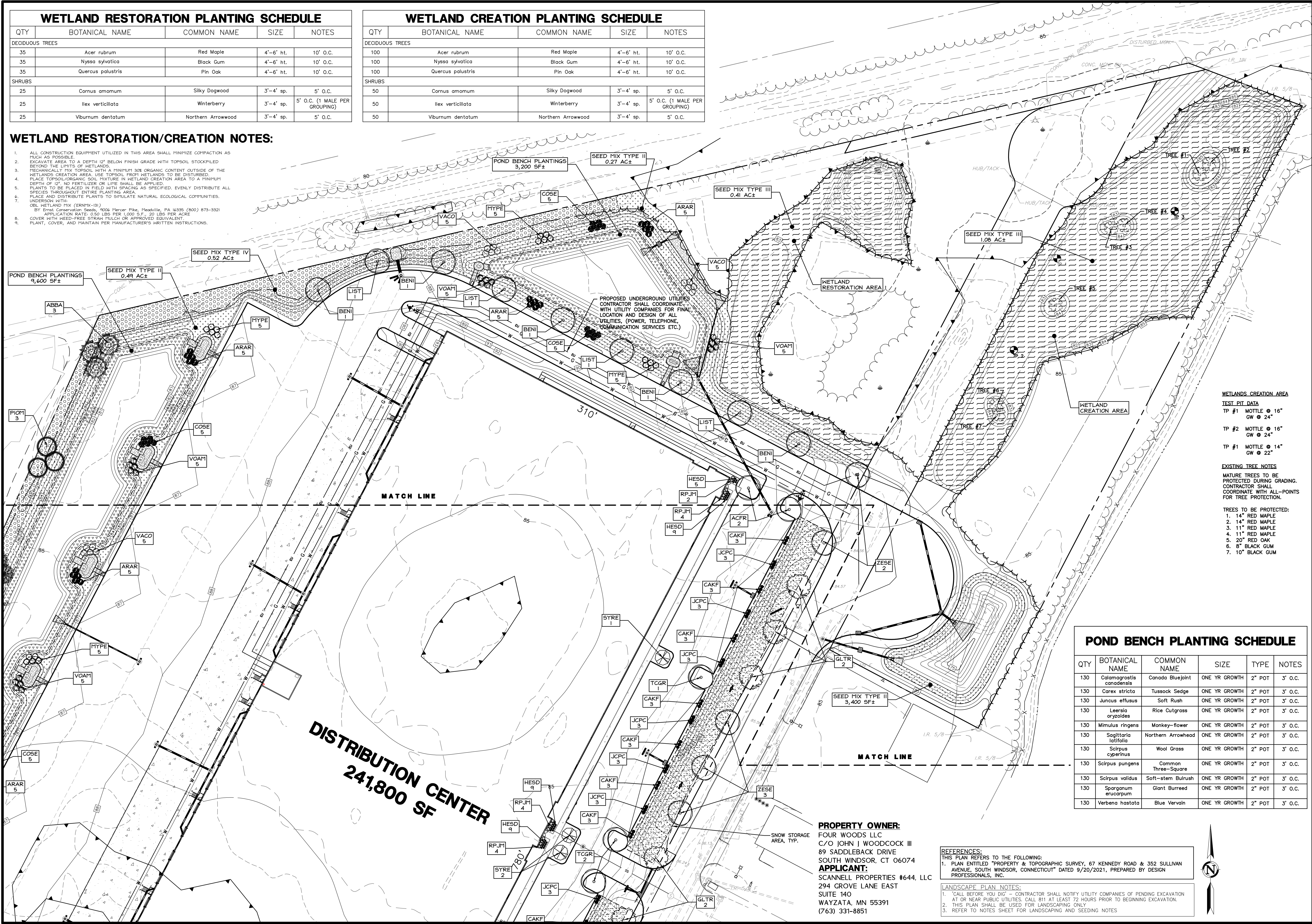
ES PLAN UPDATES
PZC SUBMISSION

EROSION & SEDIMENTATION CONTROL PLAN

SCALE: 0' 20' 40' 80'
T = 40'

SHEET
C-ES1
SHEET 11 OF 24





WETLAND RESTORATION PLANTING SCHEDULE				
QTY	BOTANICAL NAME	COMMON NAME	SIZE	NOTES
DECIDUOUS TREES				
35	Acer rubrum	Red Maple	4'-6' ht.	10' O.C.
35	Nyssa sylvatica	Black Gum	4'-6' ht.	10' O.C.
35	Quercus palustris	Pin Oak	4'-6' ht.	10' O.C.
SHRUBS				
25	Cornus amomum	Silky Dogwood	3'-4' sp.	5' O.C.
25	Ilex verticillata	Winterberry	3'-4' sp.	5' O.C. (1 MALE PER GROUPING)
25	Viburnum dentatum	Northern Arrowwood	3'-4' sp.	5' O.C.

WETLAND CREATION PLANTING SCHEDULE				
QTY	BOTANICAL NAME	COMMON NAME	SIZE	NOTES
DECIDUOUS TREES				
100	Acer rubrum	Red Maple	4'-6' ht.	10' O.C.
100	Nyssa sylvatica	Black Gum	4'-6' ht.	10' O.C.
100	Quercus palustris	Pin Oak	4'-6' ht.	10' O.C.
SHRUBS				
50	Cornus amomum	Silky Dogwood	3'-4' sp.	5' O.C.
50	Ilex verticillata	Winterberry	3'-4' sp.	5' O.C. (1 MALE PER GROUPING)
50	Viburnum dentatum	Northern Arrowwood	3'-4' sp.	5' O.C.

WETLAND RESTORATION/CREATION NOTES:

- ALL CONSTRUCTION EQUIPMENT UTILIZED IN THIS AREA SHALL MINIMIZE COMPACTION AS MUCH AS POSSIBLE.
- EXCAVATE AREA TO A DEPTH 12" BELOW FINISH GRADE WITH TOPSOIL STOCKPILED BEYOND THE LIMITS OF WETLANDS.
- Mechanically mix topsoil with a minimum 30% organic content outside of the wetlands creation area. Use topsoil from wetlands to be disturbed.
- PLACE TOPSOIL/ORGANIC SOIL MIXTURE IN WETLAND CREATION AREA TO A MINIMUM DEPTH OF 12". NO FERTILIZER OR LIME SHALL BE APPLIED.
- PLANTS TO BE PLACED IN FIELD WITH SPACING AS SPECIFIED. EVENLY DISTRIBUTE ALL SPECIES THROUGHOUT ENTIRE PLANTING AREA.
- PLACE AND DISTRIBUTE PLANTS TO SIMULATE NATURAL ECOLOGICAL COMMUNITIES.
- UNDERGO WITH WETLAND MIX (ERNIX-181)
- BY Ernst Conservation Seeds, 9006 Hancer Pike, Meadville, PA 16335 (800) 873-3521
- APPLY RATE: 0.50 LBS PER 1,000 S.F., 20 LBS PER ACRE
- COVER WITH WEED-FREE STRAW MULCH OR APPROVED EQUIVALENT.
- PLANT, COVER, AND MAINTAIN PER MANUFACTURER'S WRITTEN INSTRUCTIONS.

PROPOSED UNDERGROUND UTILITIES CONTRACTOR SHALL COORDINATE WITH UTILITY COMPANIES FOR FINAL LOCATION AND DESIGN OF ALL UTILITIES, (POWER, TELEPHONE, COMMUNICATION SERVICES ETC.)

WETLANDS CREATION AREA

TEST PIT DATA

- TP #1 MOTTLE @ 16" GW @ 24"
- TP #2 MOTTLE @ 16" GW @ 24"
- TP #1 MOTTLE @ 14" GW @ 22"

EXISTING TREE NOTES

MATURE TREES TO BE PROTECTED DURING GRADING. CONTRACTOR SHALL COORDINATE WITH ALL POINTS FOR TREE PROTECTION.

TREES TO BE PROTECTED:

1. 14" RED MAPLE
2. 14" RED MAPLE
3. 11" RED MAPLE
4. 11" RED MAPLE
5. 20" RED OAK
6. 8" BLACK GUM
7. 10" BLACK GUM

POND BENCH PLANTING SCHEDULE

QTY	BOTANICAL NAME	COMMON NAME	SIZE	TYPE	NOTES
130	Calamagrostis canadensis	Canada Bluejoint	ONE YR GROWTH	2" POT	3' O.C.
130	Carex stricta	Tussock Sedge	ONE YR GROWTH	2" POT	3' O.C.
130	Juncus effusus	Soft Rush	ONE YR GROWTH	2" POT	3' O.C.
130	Leersia oryzoides	Rice Cutgrass	ONE YR GROWTH	2" POT	3' O.C.
130	Mimulus ringens	Monkey-flower	ONE YR GROWTH	2" POT	3' O.C.
130	Sagittaria latifolia	Northern Arrowhead	ONE YR GROWTH	2" POT	3' O.C.
130	Scirpus cyperinus	Wool Grass	ONE YR GROWTH	2" POT	3' O.C.
130	Scirpus pungens	Common Three-Square	ONE YR GROWTH	2" POT	3' O.C.
130	Scirpus validus	Soft-stem Bulrush	ONE YR GROWTH	2" POT	3' O.C.
130	Sparganium angustifolium	Giant Burreed	ONE YR GROWTH	2" POT	3' O.C.
130	Verbena hastata	Blue Vervain	ONE YR GROWTH	2" POT	3' O.C.

PROPERTY OWNER:

FOUR WOODS LLC
C/O JOHN J WOODCOCK III
89 SADDLEBACK DRIVE
SOUTH WINDSOR, CT 06074
APPLICANT:
SCANNELL PROPERTIES #644, LLC
294 GROVE LANE EAST
SUITE 140
WAYZATA, MN 55391
(763) 331-8851

REFERENCES:

- PLAN ENTITLED "PROPERTY & TOPOGRAPHIC SURVEY, 67 KENNEDY ROAD & 352 SULLIVAN AVENUE, SOUTH WINDSOR, CONNECTICUT" DATED 9/20/2021, PREPARED BY DESIGN PROFESSIONALS, INC.

LANDSCAPE PLAN NOTES:

- "CALL BEFORE YOU DIG" - CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF PENDING EXCAVATION AT OR NEAR PUBLIC UTILITIES. CALL 811 AT LEAST 72 HOURS PRIOR TO BEGINNING EXCAVATION.
- THIS PLAN SHALL BE USED FOR LANDSCAPING ONLY.
- REFER TO NOTES SHEET FOR LANDSCAPING AND SEEDING NOTES.

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21 EBBETT DRIVE
P.O. BOX 167
SOUTH WINDSOR, CT 06074
860-291-8737 - F
www.designprofessionals.com

Design Professionals
CIVIL & TRAFFIC ENGINEERS / LAND SURVEYORS
PLANNERS / LANDSCAPE ARCHITECTS

PREPARED FOR:
Scannell Properties #644, LLC
294 Grove Lane East
Suite 140
Wayzata, MN 55391
763-331-8851 T

PROJECT NO.:
4670

DATE:
3/30/2022

DESIGNED BY:
DHL/BPW

CHECKED BY:
DHL/BPW

67 KENNEDY ROAD
WAREHOUSE &
DISTRIBUTION CENTER
352 SULLIVAN AVENUE, 67 & 68 KENNEDY ROAD
SOUTH WINDSOR, CONNECTICUT 06074
GIS NOS: 87300352, 49800067, & 49800068

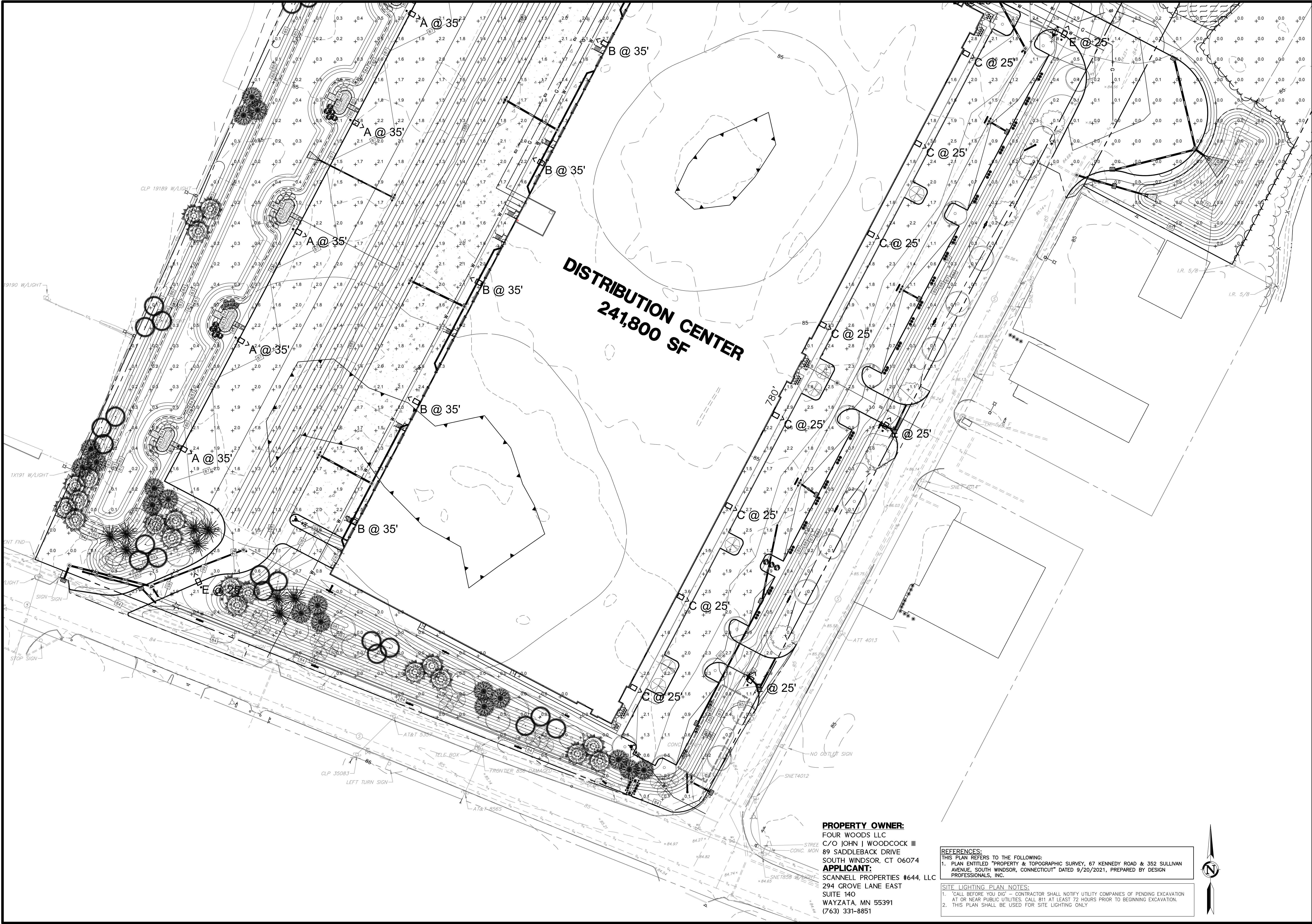
REVISIONS

NO.	DATE	BY	REVISIONS
1	4/19/22	DHL	EAS PLAN UPDATES
2	4/20/22	DHL	P2C SUBMISSION

LANDSCAPE PLAN

SHEET 15 OF 24

C-L-52



PROPERTY OWNER:
FOUR WOODS LLC
C/O JOHN J WOODCOCK III
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APPLICANT:
SCANNELL PROPERTIES #644, LLC
294 GROVE LANE EAST
SUITE 140
WAYZATA, MN 55391
(763) 331-8851

REFERENCES:
THIS PLAN REFERS TO THE FOLLOWING:
1. PLAN ENTITLED "PROPERTY & TOPOGRAPHIC SURVEY, 67 KENNEDY ROAD & 352 SULLIVAN AVENUE, SOUTH WINDSOR, CONNECTICUT" DATED 9/20/2021, PREPARED BY DESIGN PROFESSIONALS, INC.

SITE LIGHTING PLAN NOTES:
1. "CALL BEFORE YOU DIG" - CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF PENDING EXCAVATION AT OR NEAR PUBLIC UTILITIES. CALL 811 AT LEAST 72 HOURS PRIOR TO BEGINNING EXCAVATION.
2. THIS PLAN SHALL BE USED FOR SITE LIGHTING ONLY.

NO.		DATE		REVISIONS		BY	
1		4/18/22		EAS PLAN UPDATES		DHJ	
2		4/20/22		P2C SUBMISSION		DHJ	

67 KENNEDY ROAD

WAREHOUSE &

DISTRIBUTION CENTER

352 SULLIVAN AVENUE, 67 & 68 KENNEDY ROAD

SOUTH WINDSOR, CONNECTICUT 06074

GIS NOS: 87300352, 49800067, & 49800068

PREPARED FOR

Scannell Properties #644, LLC

294 Grove Lane East

Suite 140

Wayzata, MN 55391

763-331-8851 T

PROJECT NO.

4670

DATE

3/30/2022

DESIGN BY

DHJ/BPW

REVIEW BY

CONCRETE BY

CONCRETE BY

CONCRETE BY

design

professionals

CIVIL & TRAFFIC ENGINEERS / LAND SURVEYORS

PLANNERS / LANDSCAPE ARCHITECTS

21 JEFFREY DRIVE

P.O. BOX 167

SOUTH WINDSOR, CT 06074

860-291-8727 - F

860-291-8727 - F

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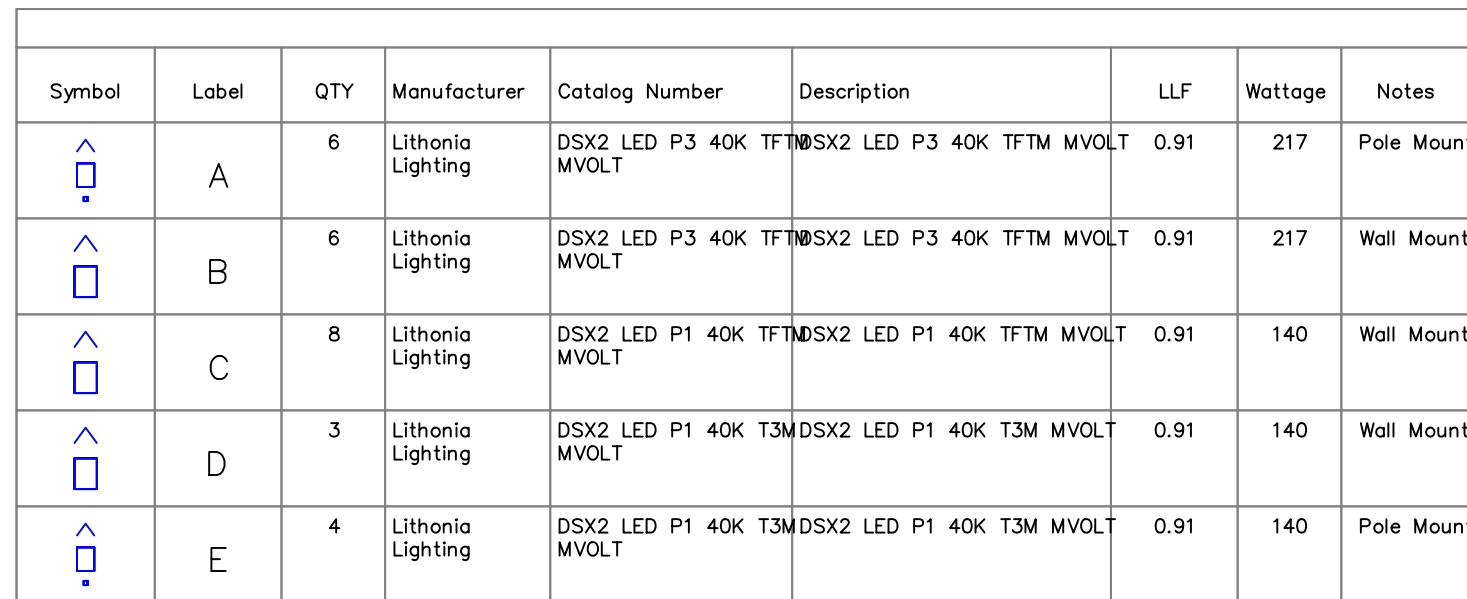
SITE LIGHTING PLAN

C-LT1

SHEET 17 OF 24

SCALE: 0' 20' 40' 80'

T = 40'

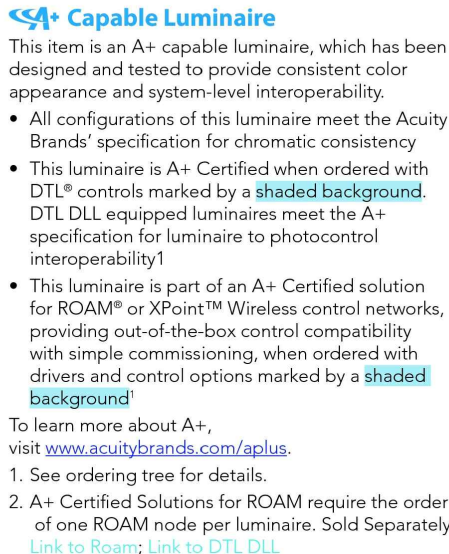


Statistics						
Description	Symbol	Avg	Max	Min	Max/Min	Avg/Min
Calc Zone #1	+	0.7 fd	3.9 fd	0.0 fd	N/A	N/A

1. THE LIGHT LEVELS SHOWN ON THESE PLANS (IN FOOTCANDLES) ARE APPROXIMATE AND BASED ON INFORMATION PROVIDED BY THE MANUFACTURER. THE CONTRACTOR SHALL VERIFY THE LIGHT LEVELS IN THE FIELD AND, TO AVOID UNDERGROUND UTILITIES, CONTRACTOR SHALL CONTACT LANDSCAPE ARCHITECT PRIOR TO INSTALLING IF DEVIATION IS 5' OR MORE FROM LOCATION SHOWN ON THE PLANS.
2. MOUNTING HEIGHT EQUALS LUMINAIRE HEIGHT ABOVE FINISHED GRADE.
3. LIGHT POLES AND/OR BASES SHALL BE MINIMUM 3' FROM FACE OF CURB.
4. SITE LIGHTS SHALL BE DESIGNED BY A QUALIFIED ELECTRICAL ENGINEER LICENSED IN THE STATE OF CONNECTICUT (BY OTHERS).
5. LIGHT POLE BASES TO BE DESIGNED BY A STRUCTURAL ENGINEER LICENSED IN THE STATE OF CONNECTICUT AND COORDINATED WITH THE LIGHTING MANUFACTURER (BY OTHERS).
6. INSTALL PER MANUFACTURER'S WRITTEN INSTRUCTIONS.

FOUR WOODS LLC
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1. CALL BEFORE YOU DIG - CONTRACTOR SHALL NOTIFY UTILITY COMPANIES OF PENDING EXCAVATION AT OR NEAR PUBLIC UTILITIES. CALL 811 AT LEAST 72 HOURS PRIOR TO BEGINNING EXCAVATION.

[illegible]

CONSTRUCTION NOTES:

1.

At least two full business days prior to starting any site activity or demolition, the contractor shall contact the applicable state utility location service by dialing 811 or submitting an online ticket request. The utilities shall be marked in all areas of proposed disturbance.
2.

It is the contractor's responsibility to review all construction contract documents associated with the project scope of work, including, but not limited to, all drawings and specifications, architectural plans, boundary and topographic survey, wetlands assessment and reports, geotechnical reports, environmental reports, and approval conditions, prior to the commencement of construction. Should the contractor find conflict and/or discrepancy between the documents relative to the plans, specifications, reports, or the relative or applicable codes, regulations, laws, rules, statutes and/or ordinances, it is the contractor's sole responsibility to notify the Engineer, in writing, of said conflict and/or discrepancy prior to the start of construction.
3.

The contractor shall be responsible for adhering to any conditions of approval placed on the project by the authorities having jurisdiction.
4.

The contractor must comply, to the fullest extent, with the latest Occupational Health and Safety (OSHA) standards and regulations, and/or any other agency with jurisdiction for construction activities. The contractor is solely responsible for construction means, methods, techniques, sequences, or procedures, or for safety precautions and programs in connection with work on the Project. The Engineer will not be responsible for the contractor's safety, schedules, or failure to carry out its work in accordance with the contract documents. The Engineer will not have control over or charge of acts or omissions of the contractor, subcontractors, or their agents or employees, or of any persons performing portions of work on the Project.
5.

Contractor must notify the Engineer in writing if there are any questions concerning the accuracy or intent of these plans or related specifications. If such notification is given, no demolition or site activity may begin until such time that the Engineer provides a written response to same.
6.

Contractor shall adhere to and is responsible for compliance with all details, notes, plans and specifications contained herein. It is the responsibility of the contractor to ensure that all work performed by their subcontractors is in full compliance with these requirements.
7.

The contractor shall confirm that they are in receipt of the current version of the referenced documents prior to the commencement of any work.
8.

Prior to commencing work, the contractor shall review and correlate all consultants plans and specifications including the entire site plan and the latest architectural plans (including, but not limited to, structural, mechanical, electrical, plumbing, and fire suppression plans, where applicable), in particular for building utility connection locations, grease trap requirements/ details, door access, and exterior grading. Contractor must immediately notify the Architect, in writing, of any conflicts, discrepancies or ambiguities which exist, and receive a written resolution prior to commencing construction.
9.

Prior to commencing work, contractor is required to secure all necessary and/or required permits and approvals for the construction of the project, including, but not limited to, demolition work, and all off site material sources and disposal facilities. Copies of all permits and approvals shall be maintained on site throughout the duration of the project. The contractor shall thoroughly review and understand all permits and permit conditions prior to fabrication of any materials or products to be used as part of the project.
10.

The contractor is responsible for independently verifying all existing onsite utilities within and adjacent to the limits of the project activities. Underground utility, structure and facility locations depicted and noted on the plans have been compiled, in part, from record mapping supplied by the respective utility companies or governmental agencies, from parol testimony, and from other sources. These locations must be considered as approximate in nature. Additionally, other such features may exist on the site, the existence of which are unknown to the Engineer.
11.

The contractor is responsible for ensuring the installation of all improvements comply with all requirements of utility companies with jurisdiction and/or control of the site.
12.

Locations of all existing and proposed services are approximate. Final utility service sizes and locations, including, but not limited to, the relocation and/or installation of utility poles, or the relocation and/or installation of transformers, are at the sole discretion of the respective utility companies.
13.

Prior to commencement of any work, the contractor shall independently coordinate and confirm with the appropriate utility companies to finalize all utility services and/or relocations to ensure no conflict with the design plans and that proper depths can be achieved. All discrepancies must immediately be reported to the Engineer in writing. Should a conflict arise due to the final designs of the utility company, the contractor shall notify the Engineer in writing and await a written resolution prior to proceeding with further utility installations.
14.

Prior to commencing construction, the contractor shall field verify all existing conditions, topographic information, utility invert elevations, and proposed layout dimensions, and must immediately notify the Engineer in writing if actual site conditions differ or are in conflict with the proposed work. No extra compensation will be paid to the contractor for work which has to be redone or repaired due to dimensions or grades shown incorrectly on these plans unless the contractor receives written permission from Owner/developer giving authorization to proceed with such additional work.
15.

Where utilities are proposed to cross/traverse existing underground utilities, the elevations of the existing utilities shall be verified in the field prior to construction by excavating a test pit at the proposed utility crossing point. Should the field verified existing utility be in conflict with the proposed site designs, the contractor shall notify the Engineer in writing and shall not proceed with said utility construction until further direction is given from the Engineer.
16.

At least 72 hours prior to starting any site activity or demolition, the contractor shall notify, at a minimum, the building official, municipal engineer, department of public works, planning and zoning commission, the Engineer, and local inland wetland commission, as applicable. The contractor shall also attend a pre-construction meeting with the local municipality, if required, prior to commencing any site activity or demolition.
17.

Prior to starting any site activity or demolition, the contractor shall implement the soil erosion and sediment control measures as noted on the plans. Refer to the Erosion and Sedimentation Control Notes.
18.

The demolition plan or existing features designated to be removed are intended to provide only general information regarding items to be demolished and/or removed. The contractor shall review all site plans (and architectural drawings as applicable) to assure that all demolition activities and incidental work necessary for the construction of the new site improvements are completed.
19.

The contractor shall protect and maintain the operation and service of all active utilities and systems that are not being removed during all construction activities. Should a temporary interruption of utility services be required as part of the proposed construction activities, the contractor shall coordinate with appropriate utility companies and the affected end users to minimize impact and service interruption.

20.

The contractor shall arrange for and coordinate with the appropriate utility companies for all services that require temporary or permanent termination for the project, whether shown on the site plans or not. Termination of utilities shall be performed in compliance with all local, state and/or federal regulations.
21.

Contractor must prepare record drawings depicting the location of existing utilities that are capped, abandoned in place, or relocated and provide to the Owner and the Engineer of record.
22.

Should hazardous material be discovered/encountered, which was not anticipated/addressed in the project plans and specifications, cease all work immediately and notify Owner and Engineer regarding the discovery of same. Do not continue work in the area until written instructions are received from an environmental professional.
23.

The contractor is responsible for preventing movement, settlement, damage, or collapse of existing structures, and any other improvements that are to remain. If any existing structures that are to remain are damaged during construction, repairs shall be made using new product/materials resulting in a pre-damage condition, or better. Contractor is responsible for all repair costs. Contractor shall document all existing damage and to notify the Owner prior to the start of construction.
24.

The use of explosives, if required, must comply with all local, state and federal regulations. The contractor shall obtain all permits that are required by the federal, state and local governments, and shall also be responsible for all notification, inspection, monitoring or testing as may be required.
25.

All debris from removal operations must be removed from the site at the time of excavation. Stockpiling of demolition debris will not be permitted. Debris shall not be burned or buried on site. All demolition materials to be disposed of, including, but not limited to, stumps, limbs, and brush, shall be done in accordance with all municipal, county, state, and federal laws and applicable codes. The contractor must maintain records of all disposal activities.
26.

The contractor is responsible for repairing all damage to any existing utilities during construction, at its own expense.
27.

All new utilities/services, including electric, telephone, cable tv, etc. are to be installed underground unless noted otherwise on the plans. The Contractor shall be responsible for installing all new utilities/services in accordance with the utility/service provider's written installation specifications and standards.
28.

All earthwork activities must be performed in accordance with these plans and specifications and the recommendations set forth in the geotechnical report completed for this project. In the absence of a geotechnical report, all earthwork activities must comply with the standard state Department of Transportation (DOT) specifications (latest edition) and any amendments or revisions thereto. All earthwork activities must comply all applicable requirements, rules, statutes, laws, ordinances and codes for the jurisdictions where the work is being performed.
29.

The contractor is responsible for removing and replacing unsuitable materials with suitable materials. All excavated or filled areas must be properly compacted. Moisture content at time of placement must be submitted in a compaction report prepared by a qualified geotechnical engineer, licensed in the state where the work is performed, verifying that all filled areas and subgrade areas within the building pad area and areas to be paved have been compacted in accordance with these plans, specifications and the recommendations. Subbase material for building pads, sidewalks, curb, or asphalt must be free of organics and other unsuitable materials. Should subbase be deemed unsuitable by Owner/developer or Owner/developer's representative, subbase is to be removed and filled with suitable material and properly compacted at the contractor's expense. All fill, compaction, and backfill materials required for utility installation must be coordinated with the applicable utility company specifications. The Engineer shall have no liability or responsibility for or as related to fill, compaction, backfill, or the balancing of earthwork.
30.

Pavement must be saw cut into straight lines and must extend to the full depth of the existing pavement, except for edge of butt joints.
31.

The tops of existing manholes, inlet structures, and sanitary cleanout tops must be adjusted as necessary, to match proposed grades.
32.

Where retaining walls (whether or not they meet the jurisdictional definition) are identified on plans, elevations identified herein are for the exposed portion of the wall. Wall footing/foundation elevations are not identified herein and are to be set/determined by the contractor based on final structural design shop drawings prepared by an appropriate professional licensed in the state where the construction occurs.
33.

Unless indicated otherwise or required by the authority having jurisdiction, all pipes shall be as follows:

Reinforced Concrete pipe (RCP) shall meet the requirements of AASHTO M 170 Class IV with silt tight joints.

High-Density Polyethylene pipe (HDPE) shall conform to AASHTO M 294, Type S (smooth interior with angular corrugations) with gaskets for silt tight joints.

Polyvinyl chloride (PVC) pipe for roof drain connections shall be SDR 35 gasket pipe. Polyvinyl Chloride (PVC) pipe for sanitary sewer pipe shall be SDR 35 gasket pipe.
34.

Storm sewer pipe lengths indicated are approximate and measured to the inside of inlet and/or manhole structure. Sanitary sewer pipe lengths indicated are approximate and measured to center of inlet and/or manhole structure to center of structure.
35.

Stormwater roof drain locations are approximate and are based on preliminary architectural plans. Contractor is responsible for reviewing and coordinating the final architectural plans to verify final locations and sizes of all roof drains.
36.

Sewers crossing streams and/or location within 10 feet of the stream embankment, or where site conditions so indicate, must be constructed of steel, reinforced concrete, ductile iron or other suitable material. Sewers conveying sanitary flow, combined sanitary and stormwater flow or industrial flow must be separated from water mains by a distance of at least 10 feet horizontally. If such lateral separations are not possible, the pipes must be in separate trenches with the sewer at least 18 inches below the bottom of the water main, or such other separation as approved by the agency with jurisdiction over same. Where appropriate separation from a water main is not possible, the sewer must be encased in concrete, or constructed of ductile iron pipe using mechanical or slip-on joints for a distance of at least 10 feet on either side of the crossing. In addition, one full length of sewer pipe should be located so both joints will be as far from the water line as possible. Where a water main crosses under a sewer, adequate structural support for the sewer must be provided.
37.

Contractor's price for water service must include all fees, costs and appurtenances required by the utility to provide full and complete working service.

38.

Contractor must contact the applicable water company to confirm the proper water meter and vault, prior to commencing construction. Water main and water service piping shall be installed in accordance with the requirements and specifications of the water authority having jurisdiction. In the absence of such specifications, water main piping must ductile iron (DIP) minimum Class 54. All work and materials must comply with the applicable American Water Works Association (AWWA) standards in effect at the time of the service application.
39.

The contractor shall ensure that all work located in existing pavement be repaired in accordance with municipal, county and/or DOT details as applicable. Contractor is responsible to coordinate the permitting, inspection and approval of completed work with the agency having jurisdiction over the proposed work.
40.

Where sump pumps are installed, all discharges must be connected to the storm sewer or discharged to an approved location.
41.

For single and multi-family residential projects, spot elevation(s) adjacent to the buildings are schematic for non-specific building footprints. Grades must be adjusted based on final architectural plans and shall provide a minimum of six (6) inches below top of foundation/concrete and/or six (6) inches below the façade treatment, whichever is lower, and must provide positive drainage away from the structure (minimum of 2%). All areas shall be graded to preclude ponding adjacent to buildings, and on or adjacent to walks/driveways leading to the buildings. All construction, including grading, must comply with all applicable building codes, local, state and federal requirements, regulations and ordinances.
42.

Contractor shall maintain and control traffic on and offsite in conformance with the current Federal Highway Administration (FHWA) "Manual on Uniform Traffic Control Devices" (MUTCD), and the federal, state, and local regulations for all aspects of demolition and site work. If a Maintenance of Traffic Plan is required for work that affects public travel either on or offsite, the contractor shall be responsible for the cost and implementation of said plan.
43.

All temporary and permanent onsite and offsite signage and pavement markings shall conform to MUTCD, ADA, state DOT, and/or local approval requirements.
44.

Contractor shall prevent the emission of dust, sediment, and debris from the site, and shall be responsible for corrective measures such as street sweeping, and clean-up work as deemed necessary by the Engineer or the authority having jurisdiction.
45.

All concrete must be air entrained with a minimum compressive strength of 4,000 psi at 28 days unless otherwise specified on the plans, details and/or geotechnical report.
46.

The Engineer will review contractor submittals which the contractor is required to submit, but only for the sole purpose of checking for general conformance with the intent of the design and contract documents. The Engineer is not responsible for any deviations from the construction documents unless contractor received explicit direction to do so, in writing, from the Engineer. The contractor remains responsible for details and accuracy, for confirming and correlating all quantities and dimensions, and for techniques of assembly and/or fabrication processes.
47.

All dimensions are to face of curb, edge of pavement, or edge of building, unless noted otherwise.
48.

The contractor shall install and/or construct all aspects of the project in strict compliance with and accordance with manufacturer's written installation standards, recommendations and specifications.

AMERICANS WITH DISABILITY ACT NOTES TO CONTRACTOR:

- The contractor shall review the proposed construction with the local building official prior to the start of construction. Contractors shall be precise in the construction of Americans with Disabilities Act (ADA) accessible parking, components, and accessible routes for the project. These components shall comply with all applicable state and local accessibility laws and regulations and the current ADA regulations and construction standards. These components include, but are not limited to the following:

•

Parking spaces and parking aisles shall not exceed a 1:50 (nominally 2.0%) slope in any direction.

•

Accessible routes shall be a minimum of 36" wide (unobstructed). Handrails and car overhangs may not obstruct these areas. Longitudinal slopes (direction of travel) shall not exceed 1:20 (5.0%) and shall have a cross slope no greater than 1:50 (2.0%).

•

Accessible routes exceeding 1:20 (5.0%) shall be considered a "ramp". Maximum slopes of a ramp shall be 1:12 (8.3%) in the direction of travel, and a cross slope of 1:50 (2.0%). Ramps shall have maximum rise of thirty (30) inches, shall be equipped with hand rails on both sides, and landings at the top and bottom of the ramp. Landings shall not exceed 1:50 (2.0%) in any direction and have positive drainage away from the landing.

•

A landing shall be provided at the exterior of all doors and at each end of ramps. Landings shall not exceed 1:50 (2.0%) in any direction and have positive drainage away from the landing and/or building. The landing shall be no less than 60 inches long unless permitted otherwise per the ADA regulations.

•

Curb ramps— shall not exceed a 1:12 (8.3%) slope for a maximum length of six (6) feet or a maximum rise of six (6) inches.

•

The contractor shall verify all existing elevations shown on the plan in areas of existing doorways, accessible routes or other areas where re-construction is proposed. The contractor shall immediately notify the Owner and Engineer in writing if any of the proposed work intended to meet ADA requirements is incapable of doing so, or if there is any ambiguity regarding which design components are intended to meet ADA requirements. The contractor shall not commence the work in the affected area until receiving written resolution from Engineer.
- | LEGEND | | |
|---|---------------------------------|----------|
| EXISTING | DESCRIPTION | PROPOSED |
| | BORING / TEST PIT LOCATION | |
| — — — — C _u — — — — C _u — | UNDERGROUND COMMUNICATION LINES | C |
| — — — — W _u — — — — W _u — | WATER MAIN | W |
| — — — — WS _u — — — — WS _u — | WATER SERVICE | WS |
| — — — — F _u — — — — F _u — | FIRE SERVICE LINE | F |
| — — — — NPW _u — — — — NPW _u — | NON-POTABLE WATER LINE | NPW |
| | WATER VALVE / FIXTURES | |
| | FIRE HYDRANT | |
| — — — — LF _u — — — — LF _u — | MAIN LIQUID FUEL LINE | LF |
| — — — — LFS _u — — — — LFS _u — | LIQUID FUEL SERVICE LINE | LFS |
| — — — — L _u — — — — L _u — | LIQUID FUEL LINE, ABANDONED | |
| — — — — I _u — — — — I _u — | IRRIGATION LINES | I |
| / | POLE / GROUND MOUNTED LIGHT | / |
| — — — — G _u — — — — G _u — | GAS MAIN | G |
| — — — — GS _u — — — — GS _u — | GAS SERVICE LINE | GS |
| — — — — E _u — — — — E _u — | ELECTRICAL LINES, OVERHEAD | EO |
| — — — — EU _u — — — — EU _u — | ELECTRICAL LINES, UNDERGROUND | EU |
| | UTILITY POLE | |
| — — — — P _u — — — — P _u — | PROPERTY LINE | |
| — — — — E _u — — — — E _u — | EASEMENT LINE | |
| | IRON PIPE | |
| | IRON ROD | |
| | MONUMENT | |
| — — — — G _u — — — — G _u — | GUARD RAIL | |
| — — — — SF — — — — SF — | SILT FENCE | SF |
| — — — — DSYL — — — — DSYL — | 4" DOUBLE SOLID YELLOW LINE | DSYL |
| — — — — SSWL — — — — SSWL — | 4" SINGLE SOLID WHITE LINE | SSWL |
| — — — — BCLC — — — — BCLC — | BIT. CONC. LIP CURB | BCLC |
| — — — — PCC — — — — PCC — | PRECAST CONCRETE CURB | PCC |
| — — — — S — — — — S — | SANITARY SEWER MAIN | S |
| — — — — SS — — — — SS — | SANITARY SEWER SERVICE LINE | SS |
| | SANITARY SEWER MANHOLE | |
| — — — — RL — — — — RL — | ROOF LEADER | RL |
| — — — — UD — — — — UD — | UNDERDRAIN | UD |
| | STORM DRAIN MANHOLE | |
| | CURB INLET | |
| | CATCH BASIN | |
| | YARD DRAIN | |
| — — — — 95 — — — — 95 — | CONTOUR | |
| — — — — X61.95 — — — — X61.95 — | SPOT ELEVATION | |
| — — — — R — — — — R — | RAMP | R |
| — — — — LSA — — — — LSA — | LANDSCAPE AREA | LSA |
- PROPERTY OWNER:**
FOUR WOODS LLC
C/O JOHN J WOODCOCK III
89 SADDLEBACK DRIVE
SOUTH WINDSOR, CT 06074
APPLICANT:
SCANNELL PROPERTIES #644, LLC
294 GROVE LANE EAST
SUITE 140
WAYZATA, MN 55391
(763) 331-8851
-
- ACCESSIBLE PARKING SYMBOL
- Not to Scale
-
- ACCESSIBLE PARKING SIGN POST
- Not to Scale
- NOTES:
1.

SIGNS SHALL BE 18 GAUGE FLAT SCREENED ALUMINUM.

2.

FOR POST MOUNTING, USE TWO HOT-DIPPED GALVANIZED MACHINE BOLTS WITH WASHERS.

3.

FOR WALL MOUNTING, USE FOUR HOT-DIPPED GALVANIZED LAG BOLTS WITH EXPANSION SHIELD.
-
- ACCESSIBLE PARKING SIGN
- Not to Scale
-
- NOTES:
1.

ACCESSIBLE PARKING SPACES AND ADA PASSENGER LOADING AREAS SHALL BE GRADED WITH A MAXIMUM SLOPE OF 1:50 (2%) IN ALL DIRECTIONS.
- VAN ACCESSIBLE PARKING SPACE
- Not to Scale
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21 EBBETT DRIVE
P.O. BOX 167
SOUTH WINDSOR, CT 06074
860-291-8727 – F
860-291-8727 – M
www.designprofessionalsinc.com

design Professionals
CIVIL & TRAFFIC ENGINEERS / LAND SURVEYORS
PLANNERS / LANDSCAPE ARCHITECTS

PREPARED FOR

Scannell Properties #644, LLC
294 Grove Lane East
Suite 140
Wayzata, MN 55391
763-331-8851 T

PROJECT NO.

4670

DATE

3/30/2023

ISSUED BY

DHJ/BPW

DESIGNED BY

DHJ

CHECKED BY

CPW

67 KENNEDY ROAD
WAREHOUSE &
DISTRIBUTION CENTER
352 SULLIVAN AVENUE, 67 & 68 KENNEDY ROAD
SOUTH WINDSOR, CONNECTICUT 06074
GIS NOS: 87300352, 49800067, & 49800068

REVISIONS

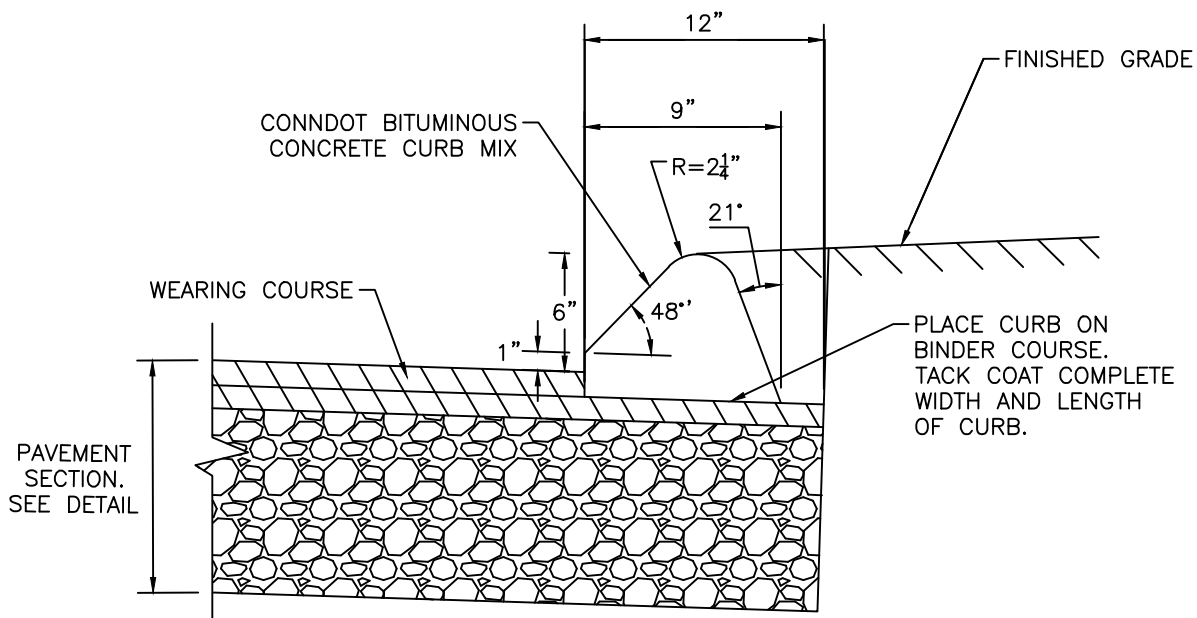
NO.	DATE	BY
1	4/7/22	DHJ
2	4/20/22	DHJ

NOTES, LEGEND, & DETAILS

SHEET

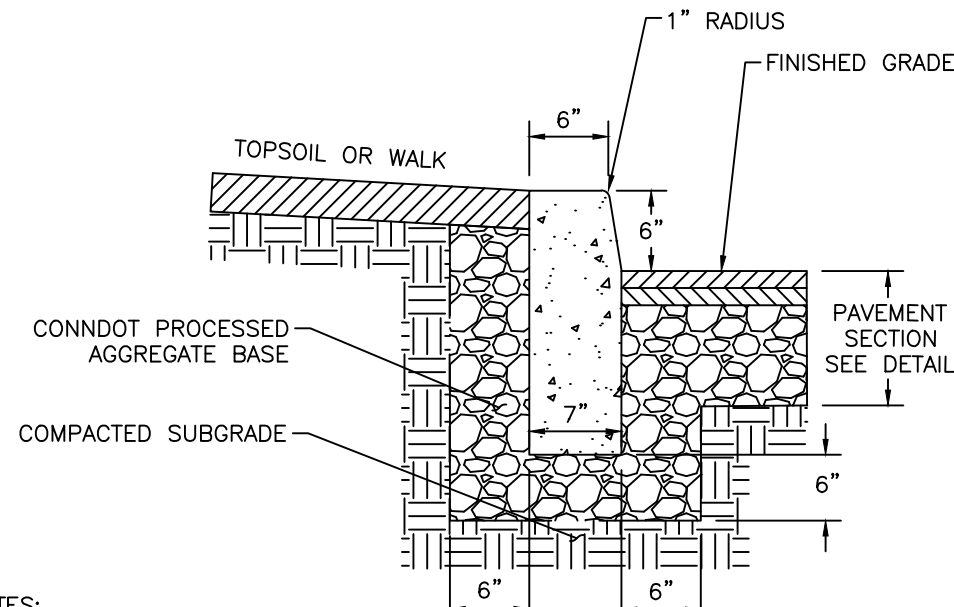
C-D1

SHEET 19 OF 24



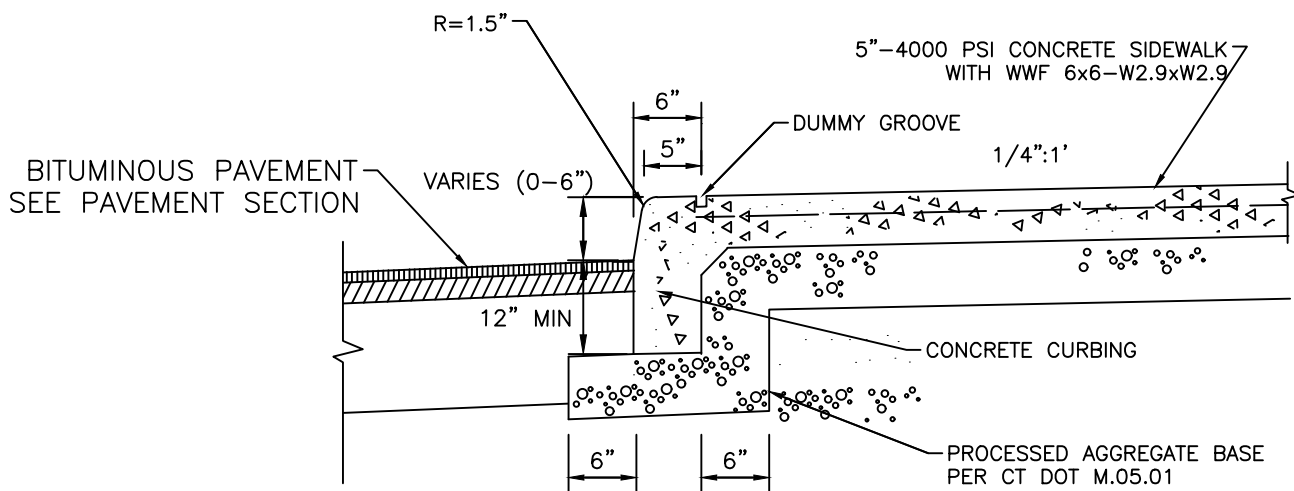
NOTE:
BITUMINOUS CONCRETE CURBING SHALL BE INSTALLED PER CONNDOT STANDARD SPECIFICATIONS FOR ROADS, BRIDGES, FACILITIES AND INCIDENTAL CONSTRUCTION, LATEST EDITION.

1 BITUMINOUS CONCRETE LIP CURB Not to Scale



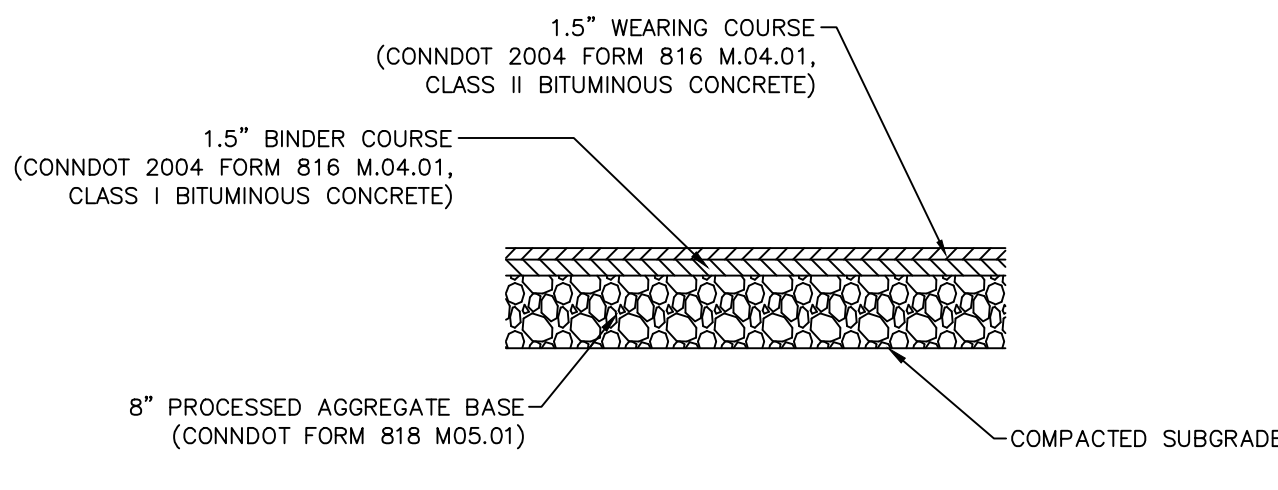
- NOTES:
1. ALL CONCRETE SHALL BE CONNDOT CLASS 'F' OR EQUIVALENT.
 2. THE END OF CURB SECTIONS SHALL BE CHAMFERED $\frac{1}{4}$ ".
 3. CURB, CURB CORNERS OR EDGING SHALL MATCH THE ADJACENT CURB IN SIZE, COLOR AND FINISH.
 4. CURBS, CURB CORNERS OR EDGING SHALL BE FITTED TOGETHER AS CLOSELY AS POSSIBLE.
 5. EXPANSION JOINTS SHALL BE INSTALLED AT A MAXIMUM OF 20 FEET ON CENTER USING PREFORMED EXPANSIONS JOINT FILLER HAVING A THICKNESS OF $\frac{3}{8}$ ".
 6. WHEN ABUTTING CONCRETE WALKS, INSTALL PREFORMED EXPANSIONS JOINT FILLER HAVING A THICKNESS OF $\frac{3}{8}$ " ALONG LENGTH OF WALK.
 7. CONCRETE CURBING SHALL BE INSTALLED PER CONNDOT STANDARD SPECIFICATIONS FOR ROADS, BRIDGES, FACILITIES AND INCIDENTAL CONSTRUCTION, LATEST EDITION.

2 PRECAST CONCRETE CURB Not to Scale



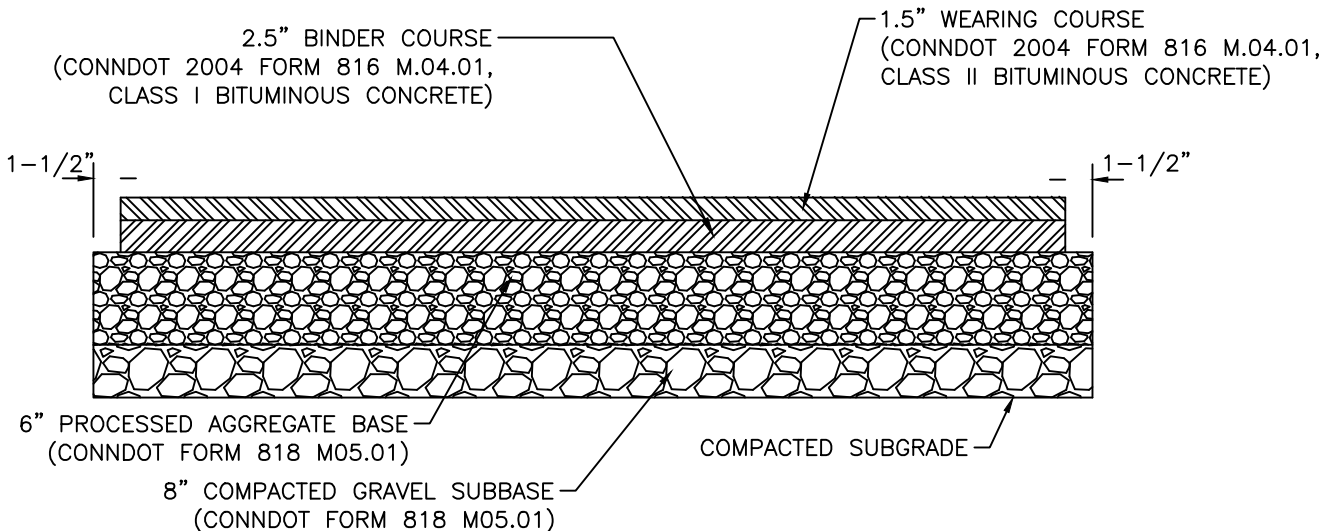
- NOTES:
1. ALL CONCRETE FOR SIDEWALKS SHALL BE CLASS 'F'. MEET CONNECTICUT D.O.T. SPECIFICATIONS. REFER TO FORM 817.
 2. CONCRETE SURFACE TO BE SCORED AT 5 FOOT INTERVALS.
 3. EXPANSION JOINTS SHALL BE INSTALLED EVERY 15 FEET.
 4. PROVIDE BROOMED FINISH PERPENDICULAR TO TRAVEL PATH

3 MONOLITHIC CONCRETE WALK AND CURB Not to Scale



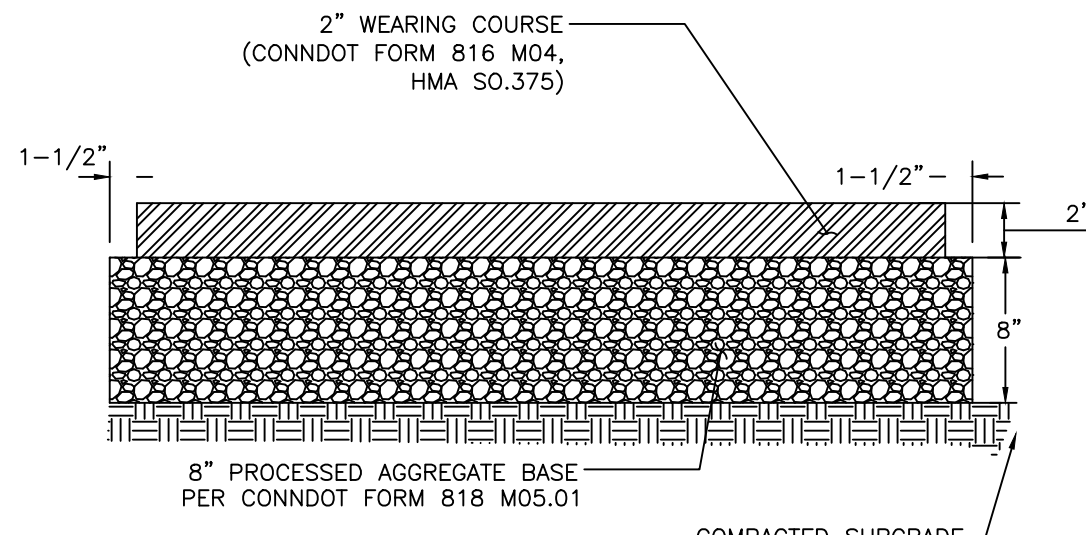
- NOTES:
1. DETAIL IS PREPARED FOR PLANNING PURPOSES ONLY. IN THE ABSENCE OF A GEOTECHNICAL REPORT, CONTRACTOR SHALL COORDINATE WITH OWNER AND DEVELOPER TO OBTAIN OR DETERMINE IF GEOTECHNICAL PAVEMENT AND SECTION DESIGNS ARE REQUIRED.

4 BITUMINOUS CONCRETE STANDARD DUTY PAVEMENT Not to Scale



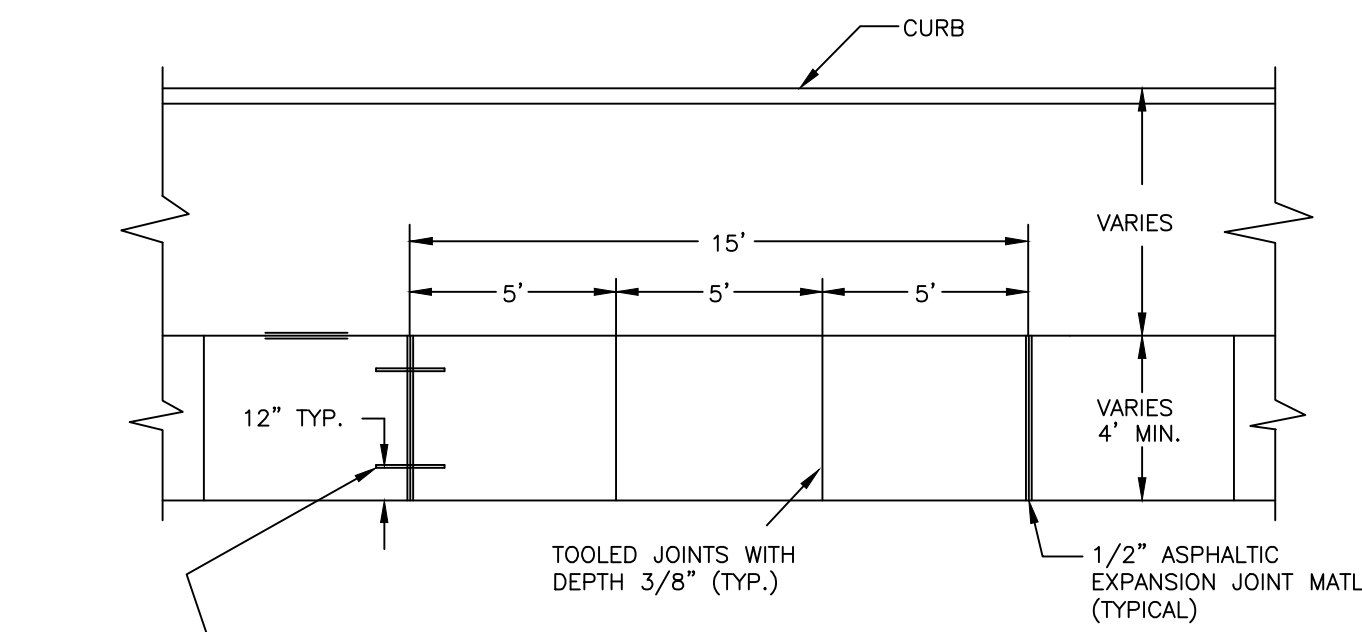
- NOTES:
1. HEAVY DUTY PAVEMENT AREA NOTED ON SITE PLAN. ALL OTHER PAVEMENT AREAS TO BE STANDARD DUTY.
 2. DETAIL IS PREPARED FOR PLANNING PURPOSES ONLY. IN THE ABSENCE OF A GEOTECHNICAL REPORT, CONTRACTOR SHALL COORDINATE WITH OWNER AND DEVELOPER TO OBTAIN OR DETERMINE IF GEOTECHNICAL PAVEMENT AND SECTION DESIGNS ARE REQUIRED.

5 BITUMINOUS CONCRETE HEAVY DUTY PAVEMENT Not to Scale

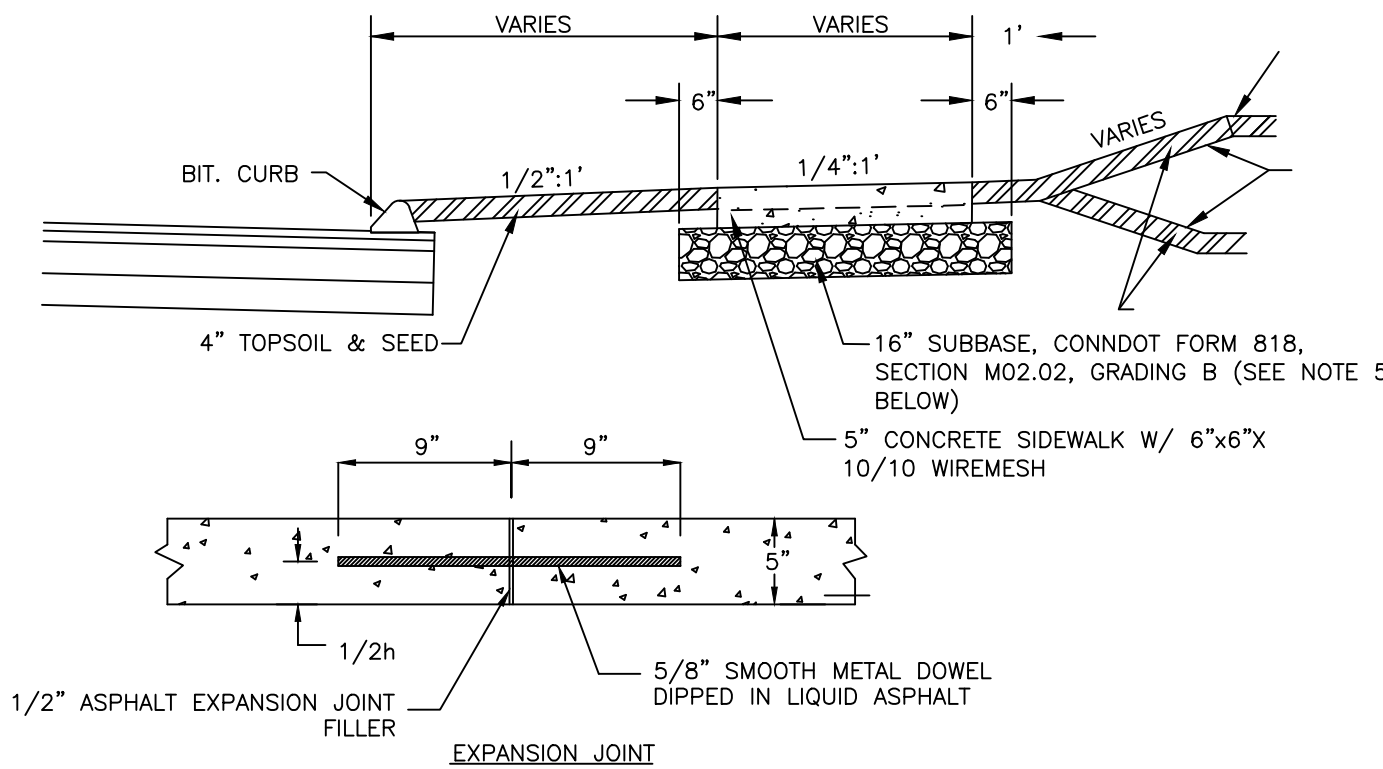


- NOTES:
1. SEE SHEET SITE PLANS FOR LAYOUT AND DIMENSIONS.
 2. FOR WET SUBGRADE CONDITIONS THERE MAY BE A REQUIREMENT TO PLACE AN INITIAL 10+\"/>

6 BITUMINOUS CONCRETE SIDEWALK Not to Scale

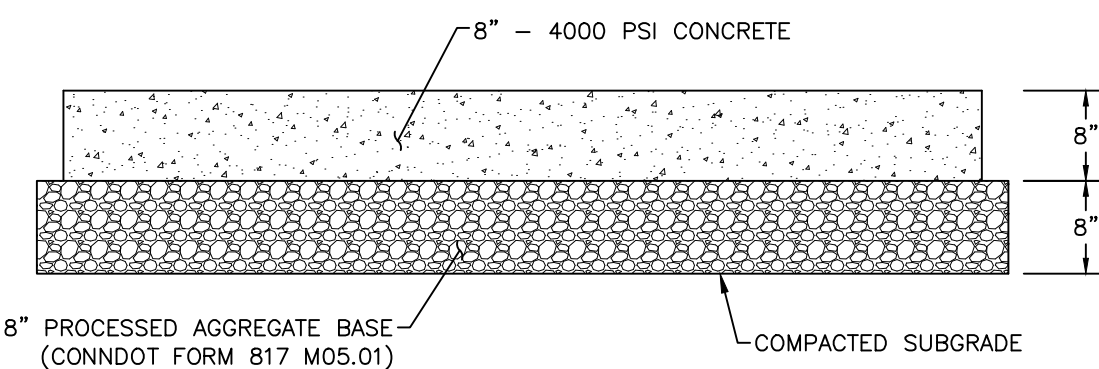


NOTE: DOWELS SHALL BE PLACED AT CONSTRUCTION JOINTS AND WHERE NEW CONCRETE MEETS EXISTING CONCRETE (SEE DETAIL).



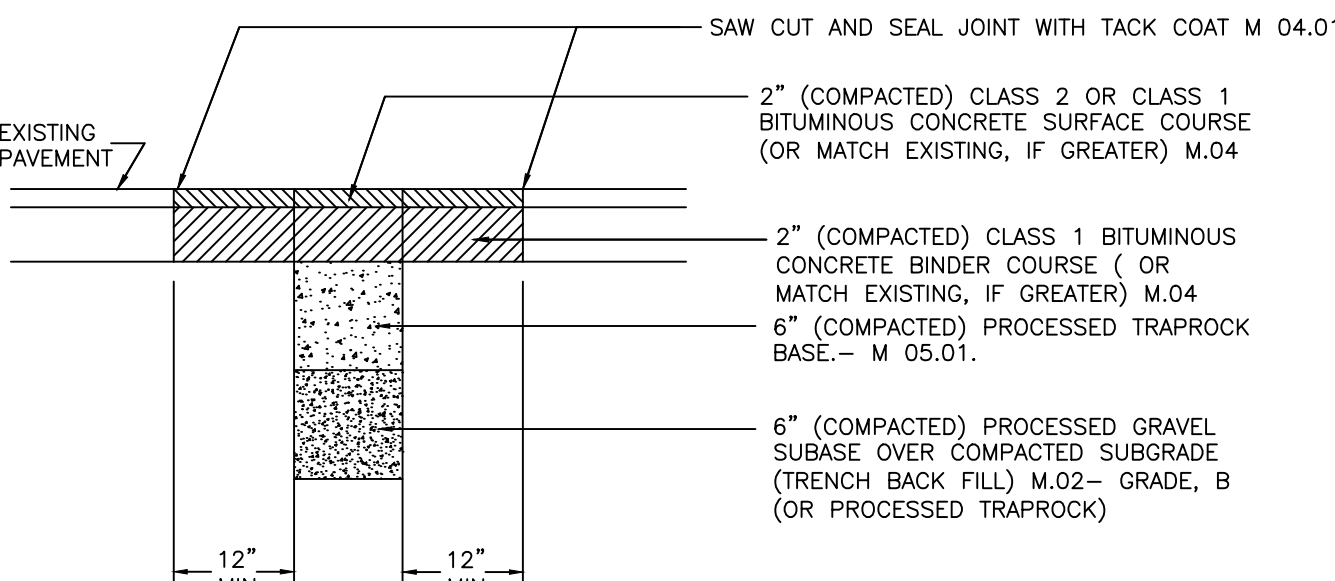
- NOTES:
1. ALL CONCRETE FOR SIDEWALKS SHALL BE CLASS 'F'. MEET CONNECTICUT D.O.T. SPECIFICATIONS. REFER TO FORM 818.
 2. CONCRETE SURFACE TO BE SCORED AT 5 FOOT INTERVALS.
 3. EXPANSION JOINTS SHALL BE INSTALLED EVERY 15 FEET.
 4. PROVIDE BROOMED FINISH PERPENDICULAR TO TRAVEL PATH.
 5. SUBBASE DEPTH SHALL BE 24\"/>

7 CONCRETE SIDEWALK Not to Scale



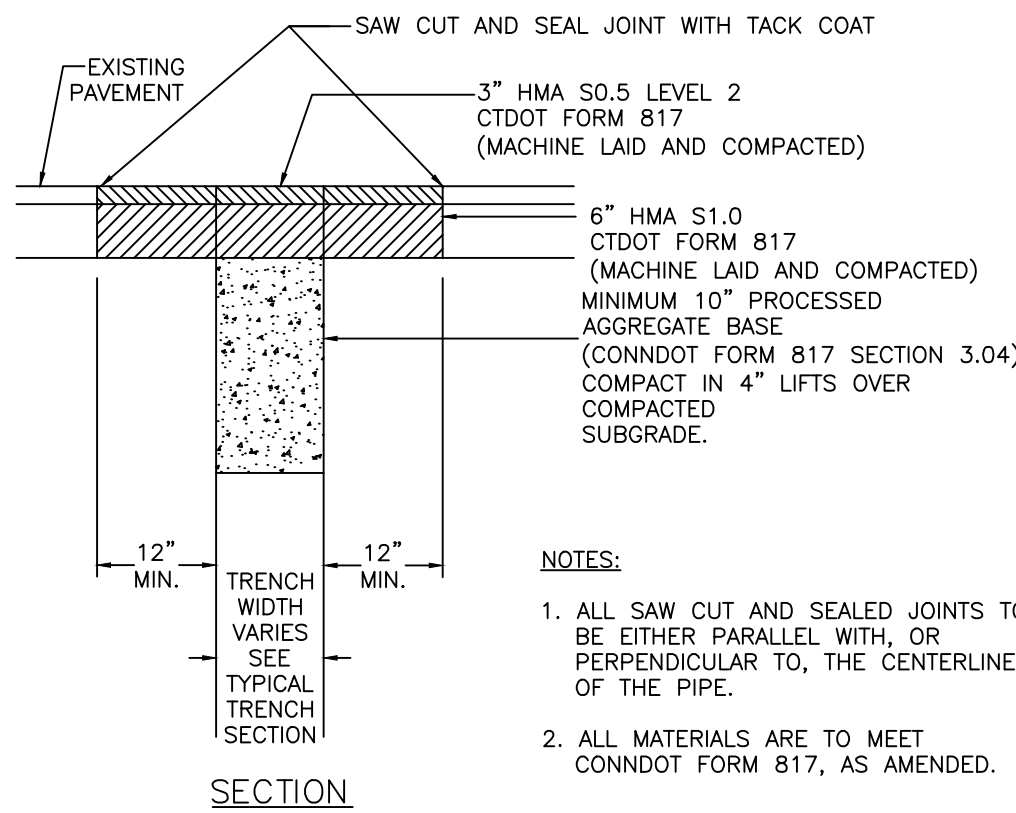
- NOTES:
1. DETAIL IS PREPARED FOR PLANNING PURPOSES ONLY. IN THE ABSENCE OF A GEOTECHNICAL REPORT, CONTRACTOR SHALL COORDINATE WITH OWNER AND DEVELOPER TO OBTAIN OR DETERMINE IF GEOTECHNICAL PAVEMENT AND SECTION DESIGNS ARE REQUIRED.
 2. CONCRETE SHALL BE MIXED PER SPECIFICATION OUTLINED IN CONNDOT FOR 817 M.03.

8 CONCRETE DOCK APRON Not to Scale



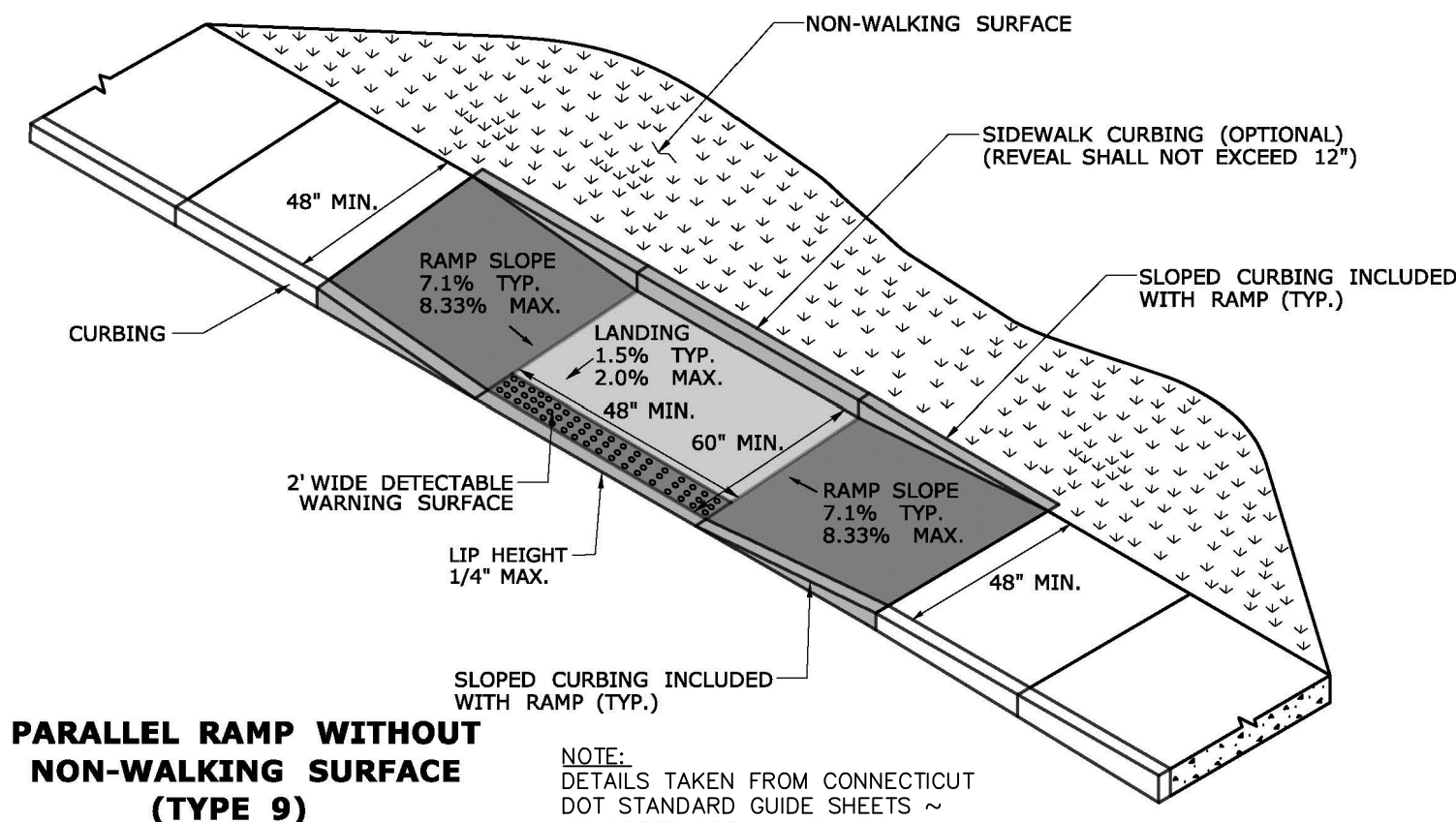
- NOTES:
1. ALL SAW CUT AND SEALED JOINTS TO BE EITHER PARALLEL WITH, OR PERPENDICULAR TO, THE CENTERLINE OF THE PIPE.
 2. ALL MATERIALS ARE TO MEET FORM 817, AS AMENDED

9 PERMANENT PAVEMENT PATCHING (KENNEDY ROAD) Not to Scale



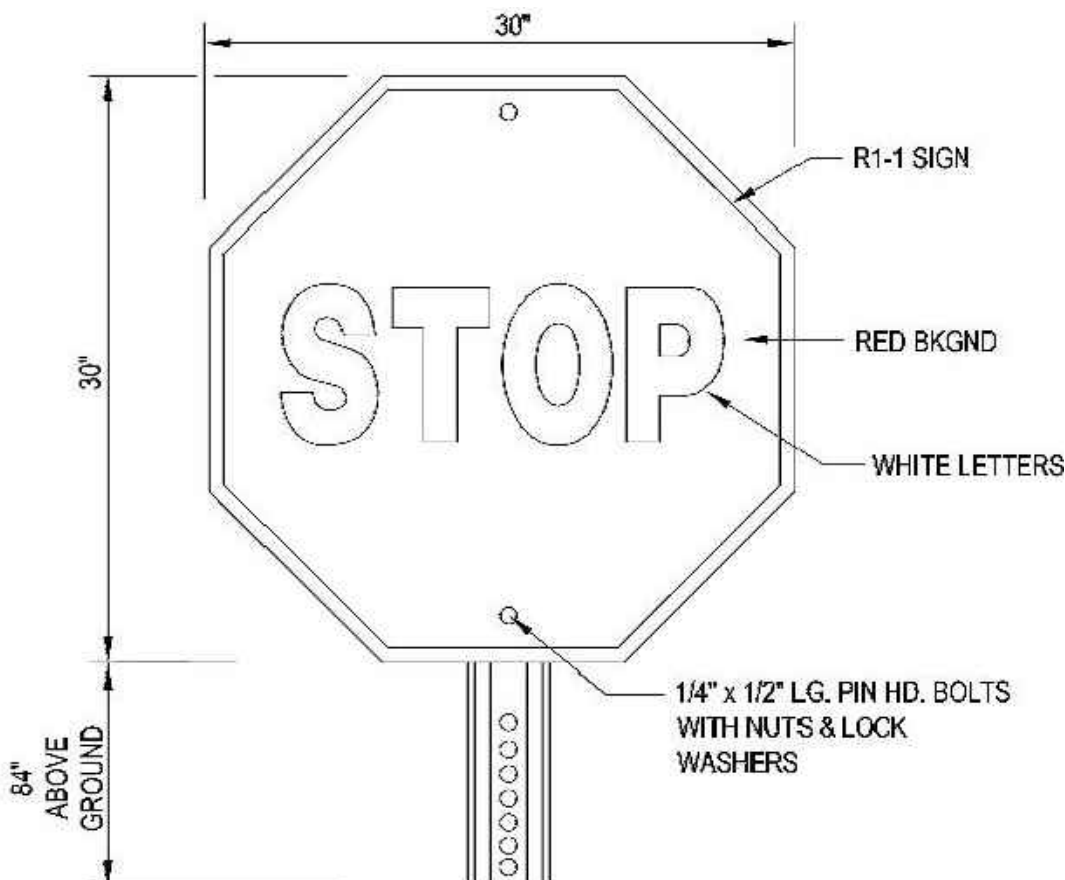
- NOTES:
1. ALL SAW CUT AND SEALED JOINTS TO BE EITHER PARALLEL WITH, OR PERPENDICULAR TO, THE CENTERLINE OF THE PIPE.
 2. ALL MATERIALS ARE TO MEET CONNDOT FORM 817, AS AMENDED.

10 ROUTE 30 PERMANENT PAVEMENT PATCHING (SULLIVAN AVE) Not to Scale

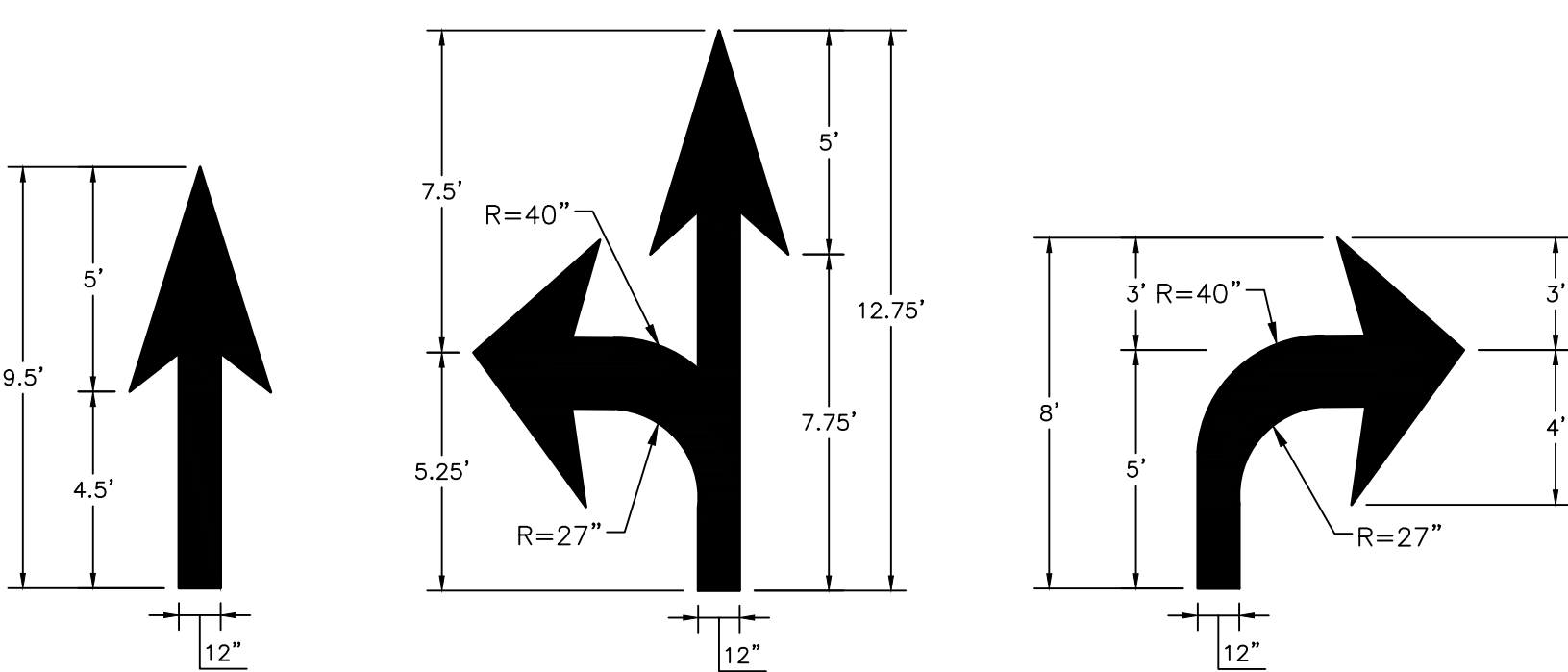


NOTE: DETAILS TAKEN FROM CONNECTICUT DOT STANDARD GUIDE SHEETS ~ CONCRETE SIDEWALK RAMPS ~ ISSUED NOVEMBER 22, 2021

11 CONCRETE SIDEWALK RAMP Not to Scale

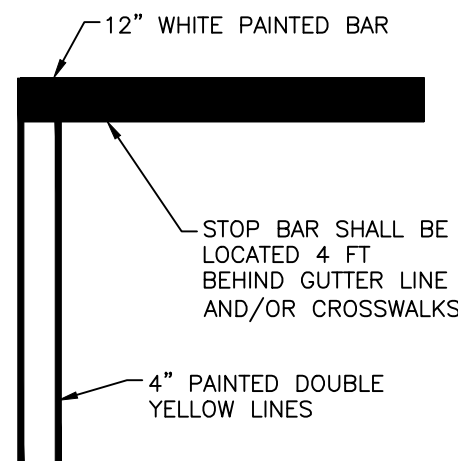


12 STOP SIGN Not to Scale

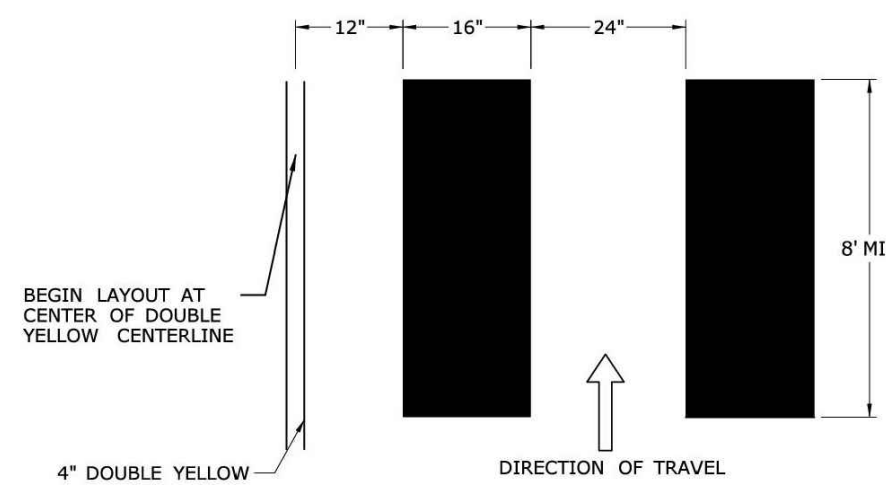


- NOTE:
1. PAVEMENT TRAFFIC ARROW DIMENSIONS TAKEN FROM CTDOT STANDARD SHEET "TR-1210_04: PAVEMENT MARKING LINES AND SYMBOLS".

13 TRAFFIC ARROWS Not to Scale



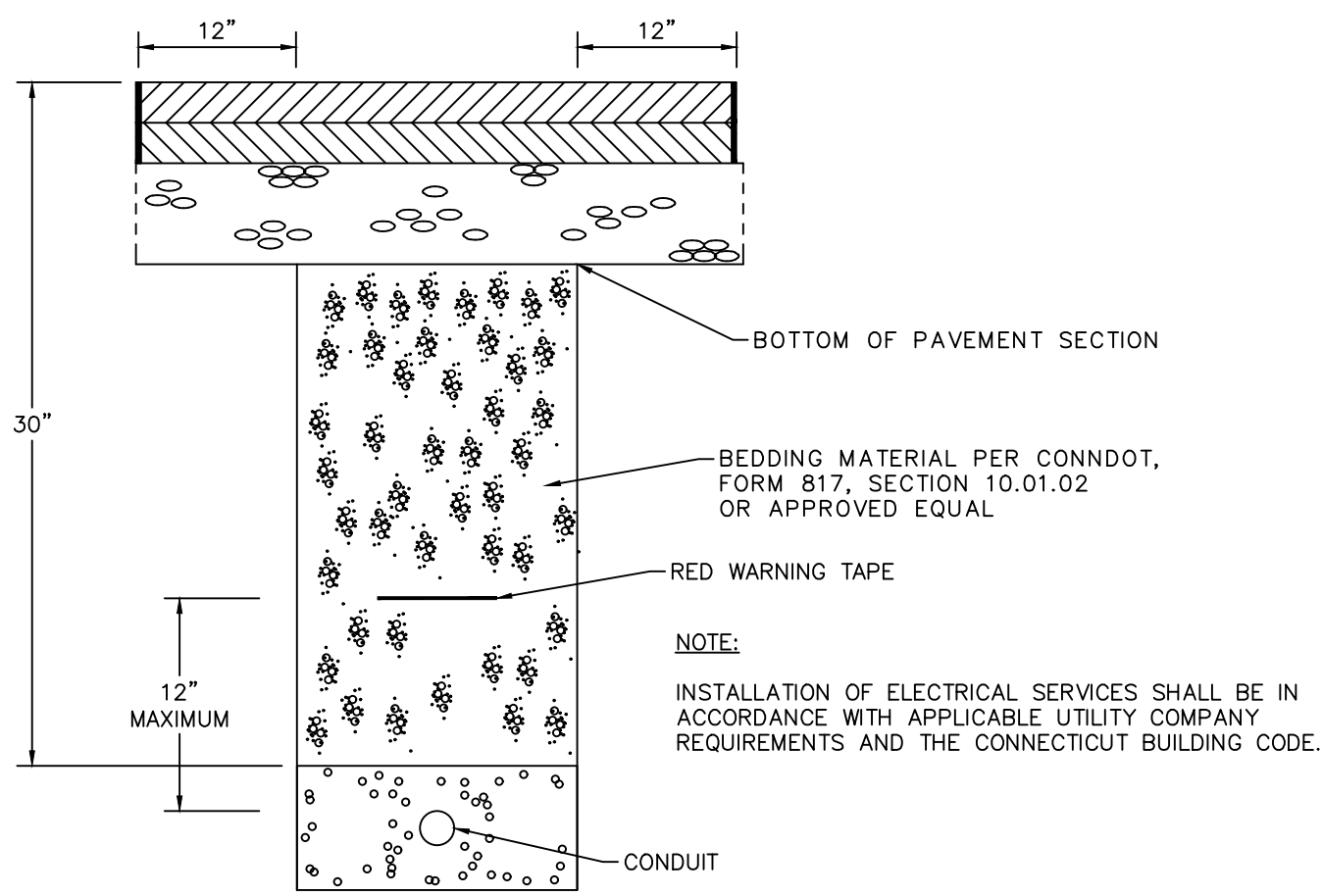
14 STOP BAR Not to Scale



- NOTE:
1. CROSSWALK LINES SHALL BE PARALLEL TO VEHICULAR DIRECTION OF TRAVEL.
 2. CROSSWALK DETAIL TAKEN FROM CTDOT STANDARD SHEET "TR-1210_04: PAVEMENT MARKING LINES AND SYMBOLS".

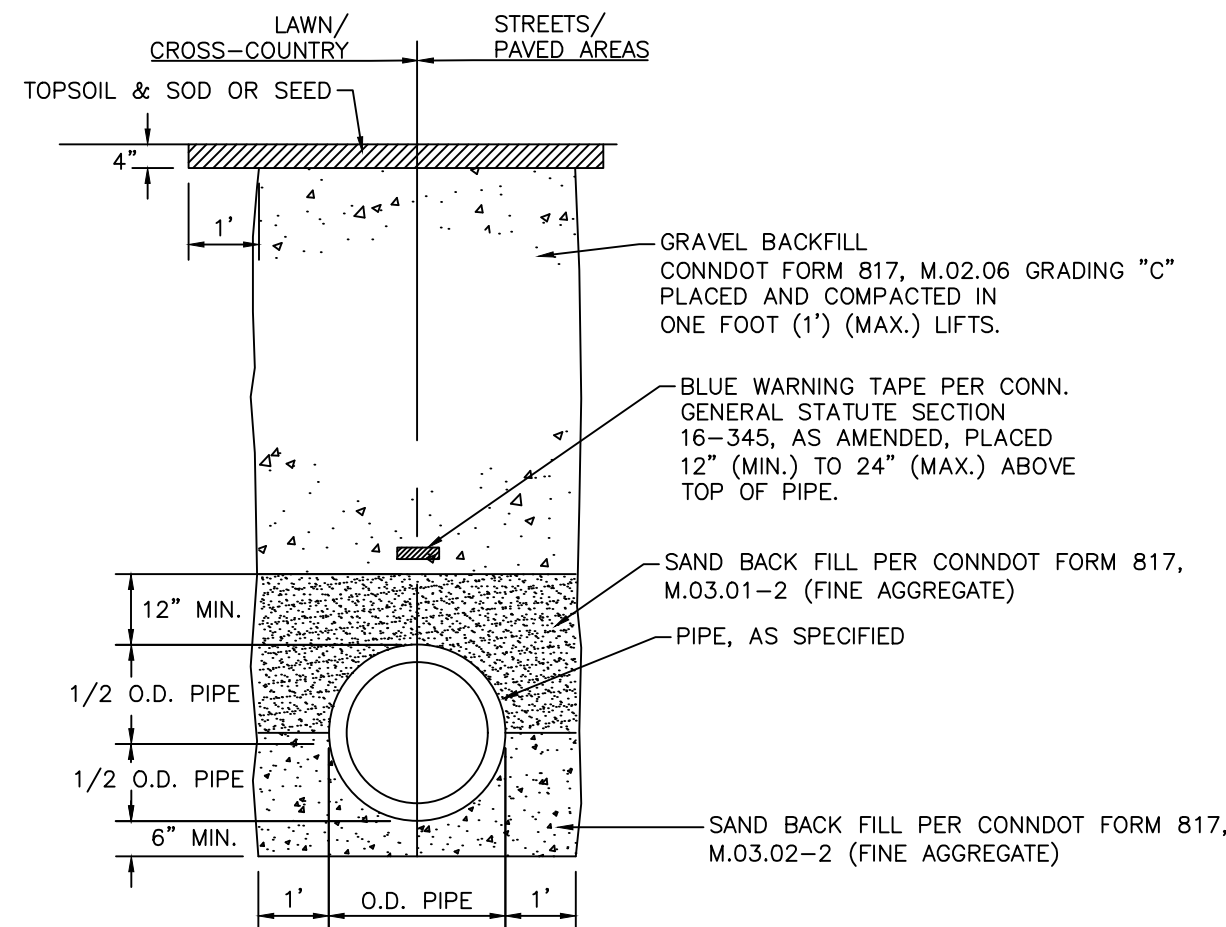
15 CROSSWALK Not to Scale

PROPERTY OWNER:
FOUR WOODS LLC
C/O JOHN J WOODCOCK III
89 SADDLEBACK DRIVE
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APPLICANT:
SCANNELL PROPERTIES #644, LLC
294 GROVE LANE EAST
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WAYZATA, MN 55391
(763) 331-8851



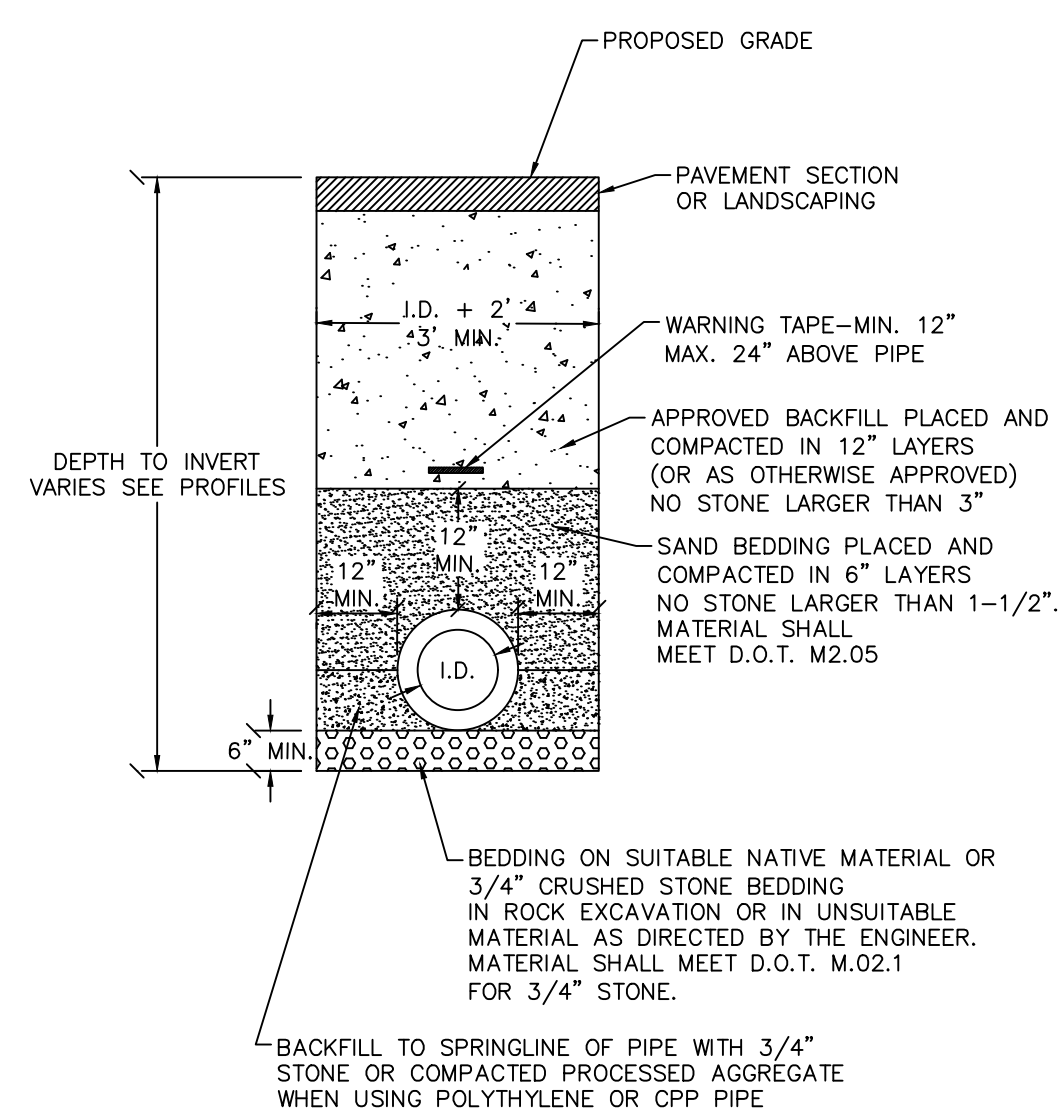
1 ELECTRIC TRENCH SECTION

Not to Scale



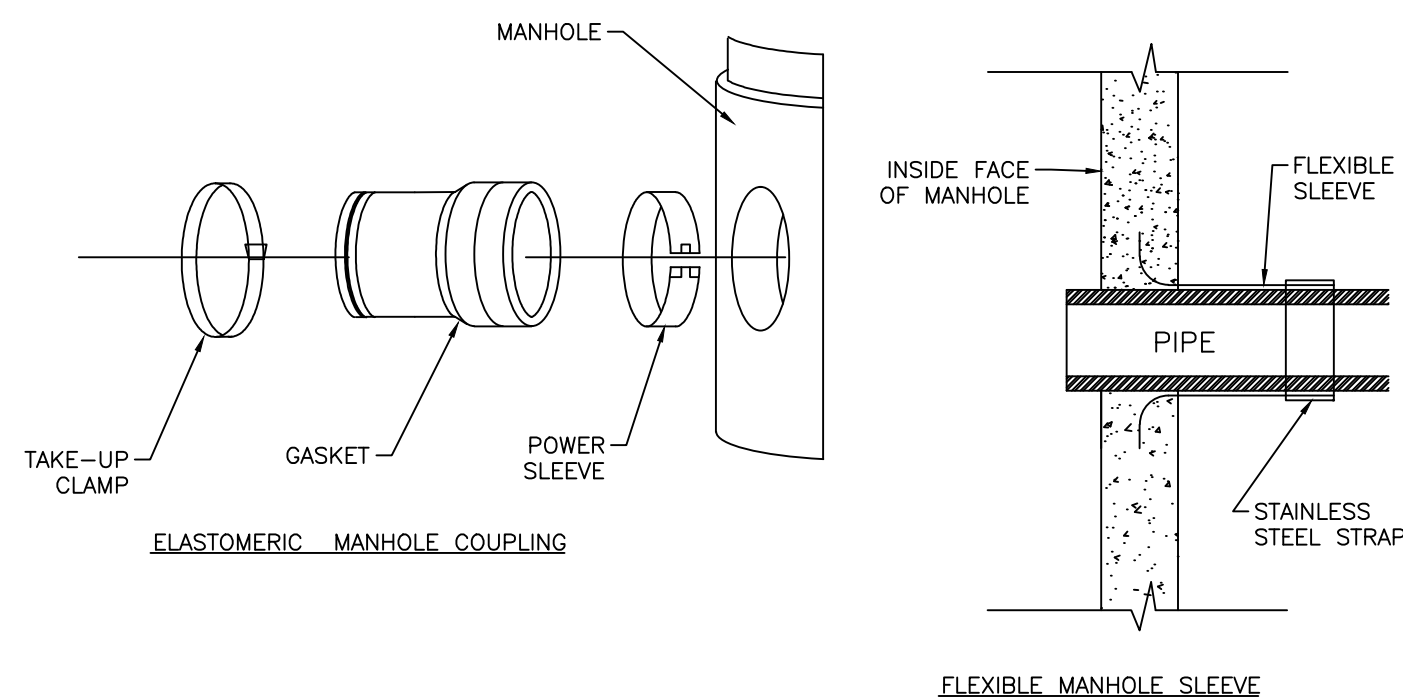
2 WATER TRENCH SECTION

Not to Scale



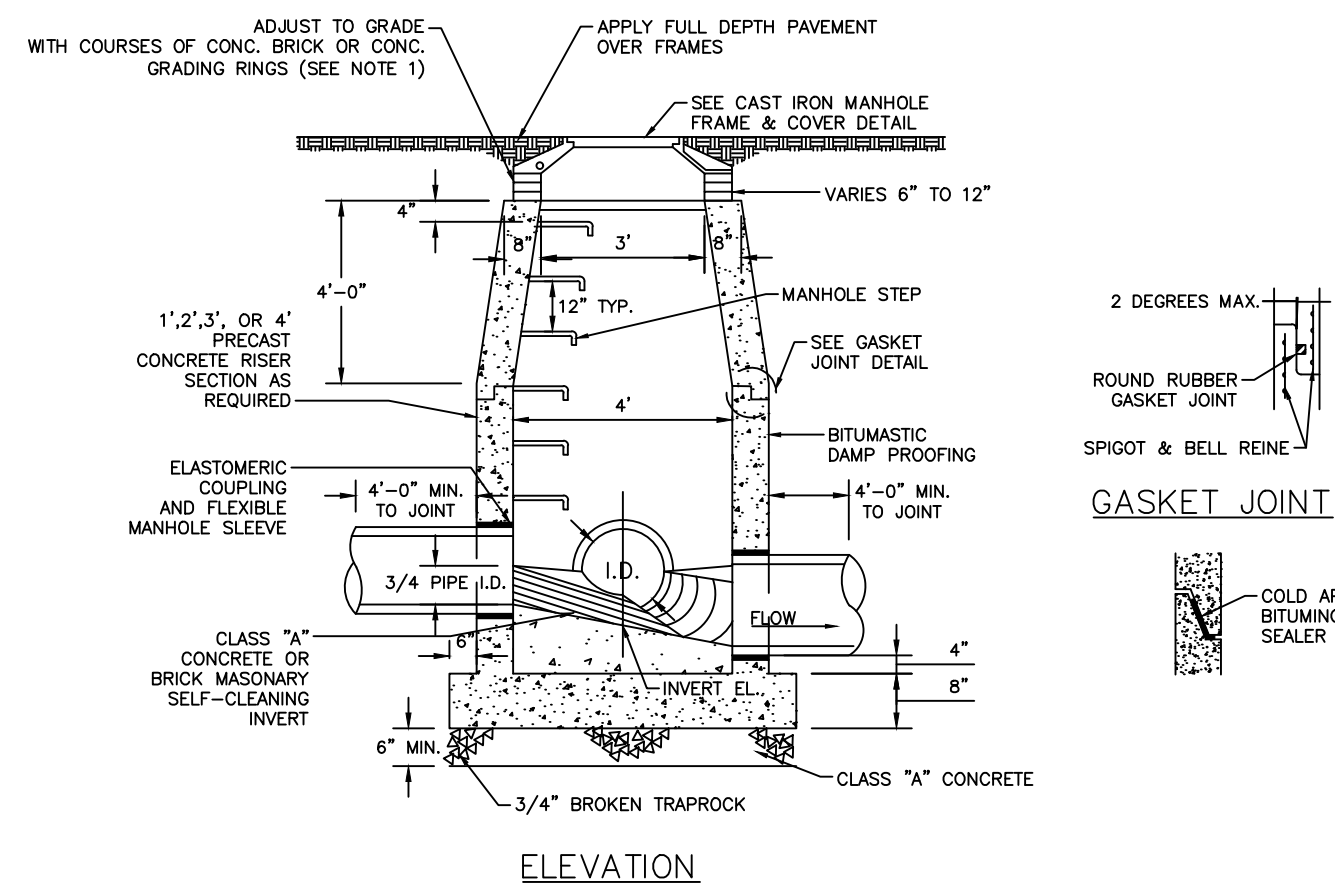
3 STORM SEWER TRENCH SECTION

Not to Scale



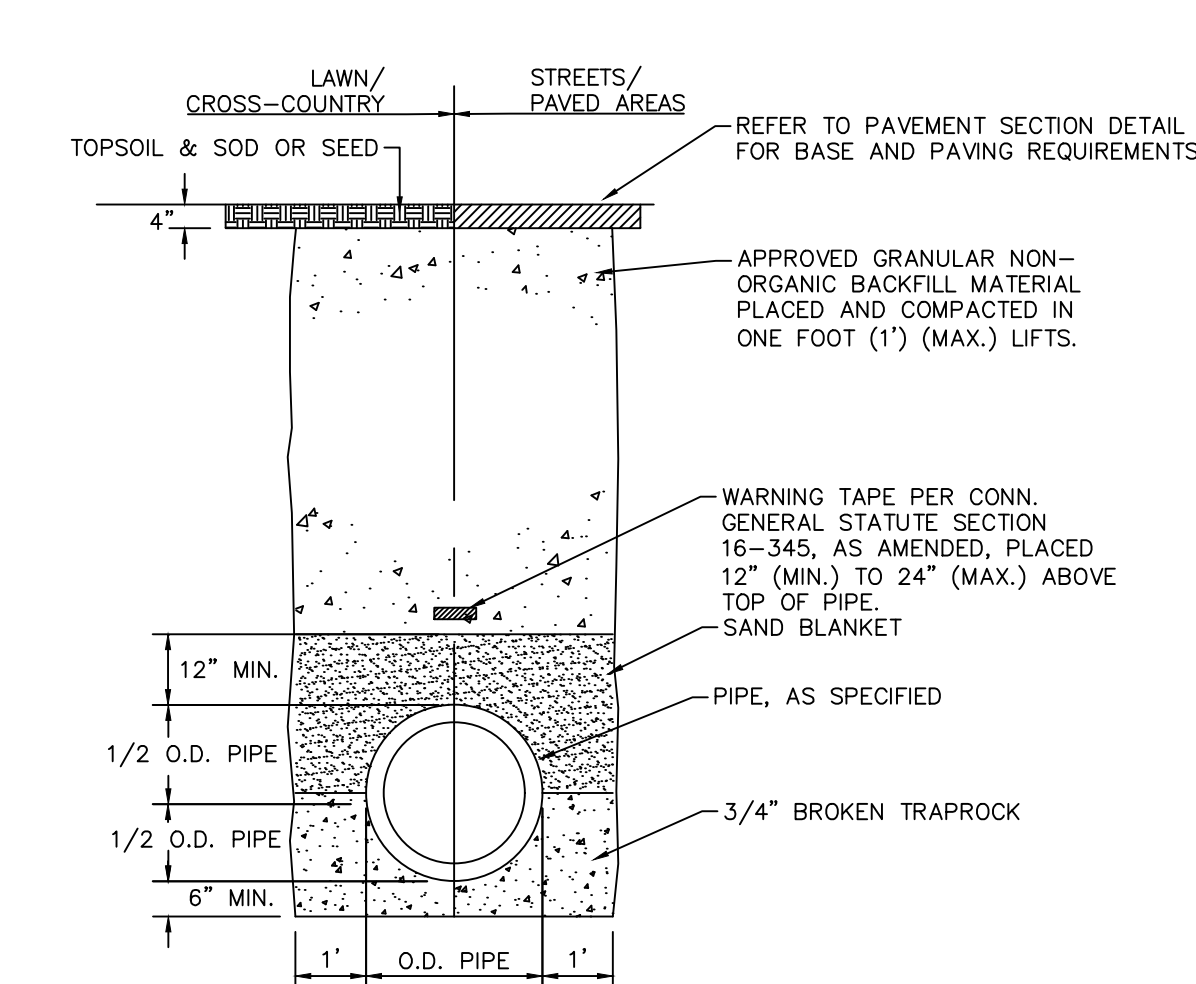
4 SANITARY LATERAL CONNECTION TO MANHOLE

Not to Scale



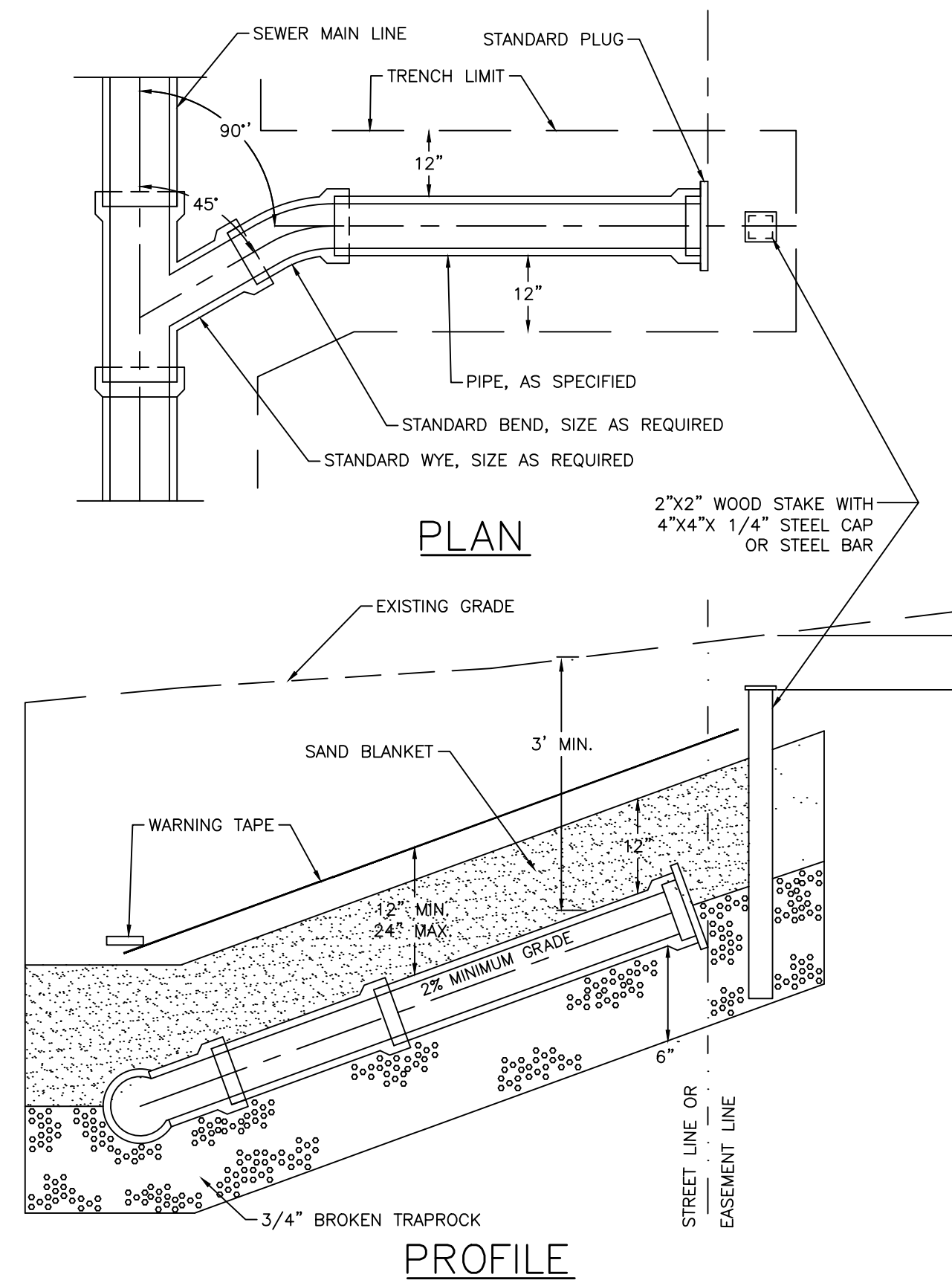
5 SANITARY MANHOLE

Not to Scale



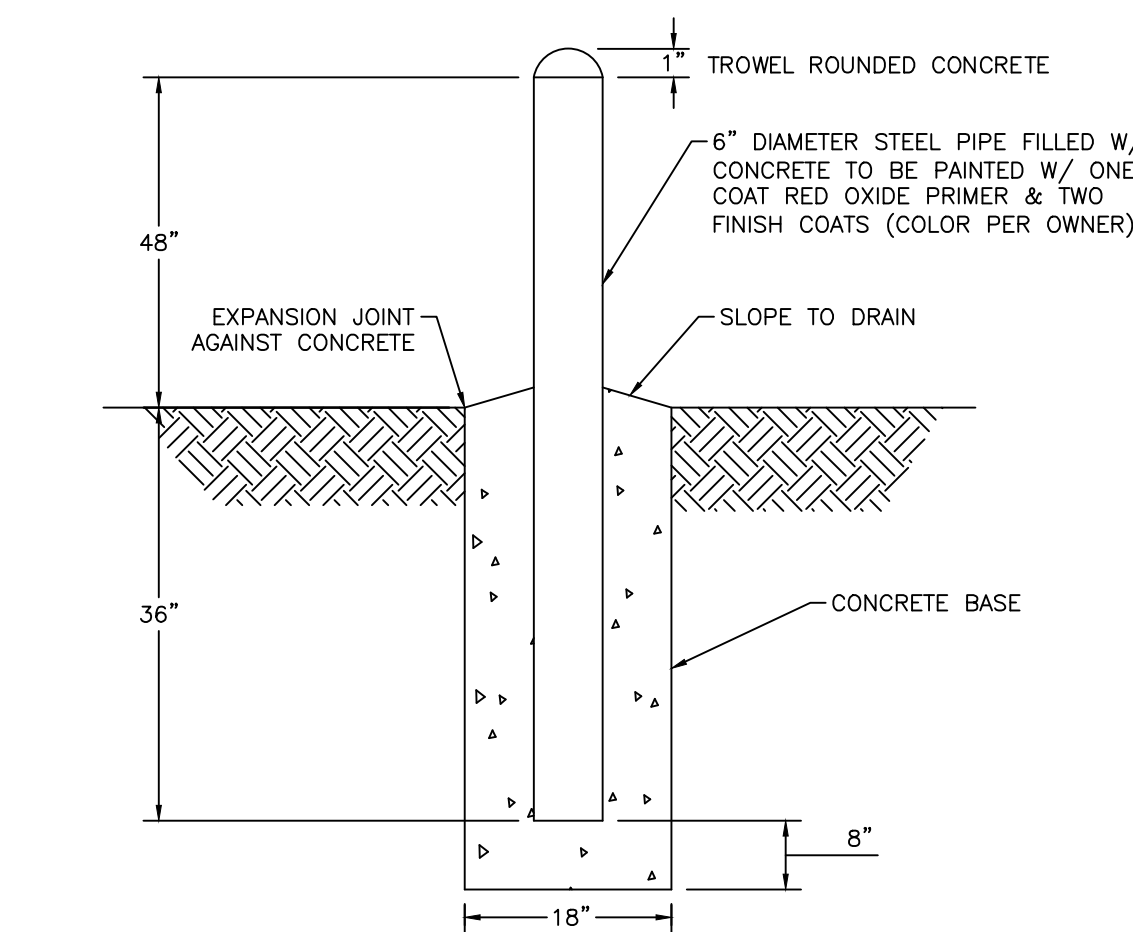
6 SANITARY SEWER TRENCH SECTION

Not to Scale



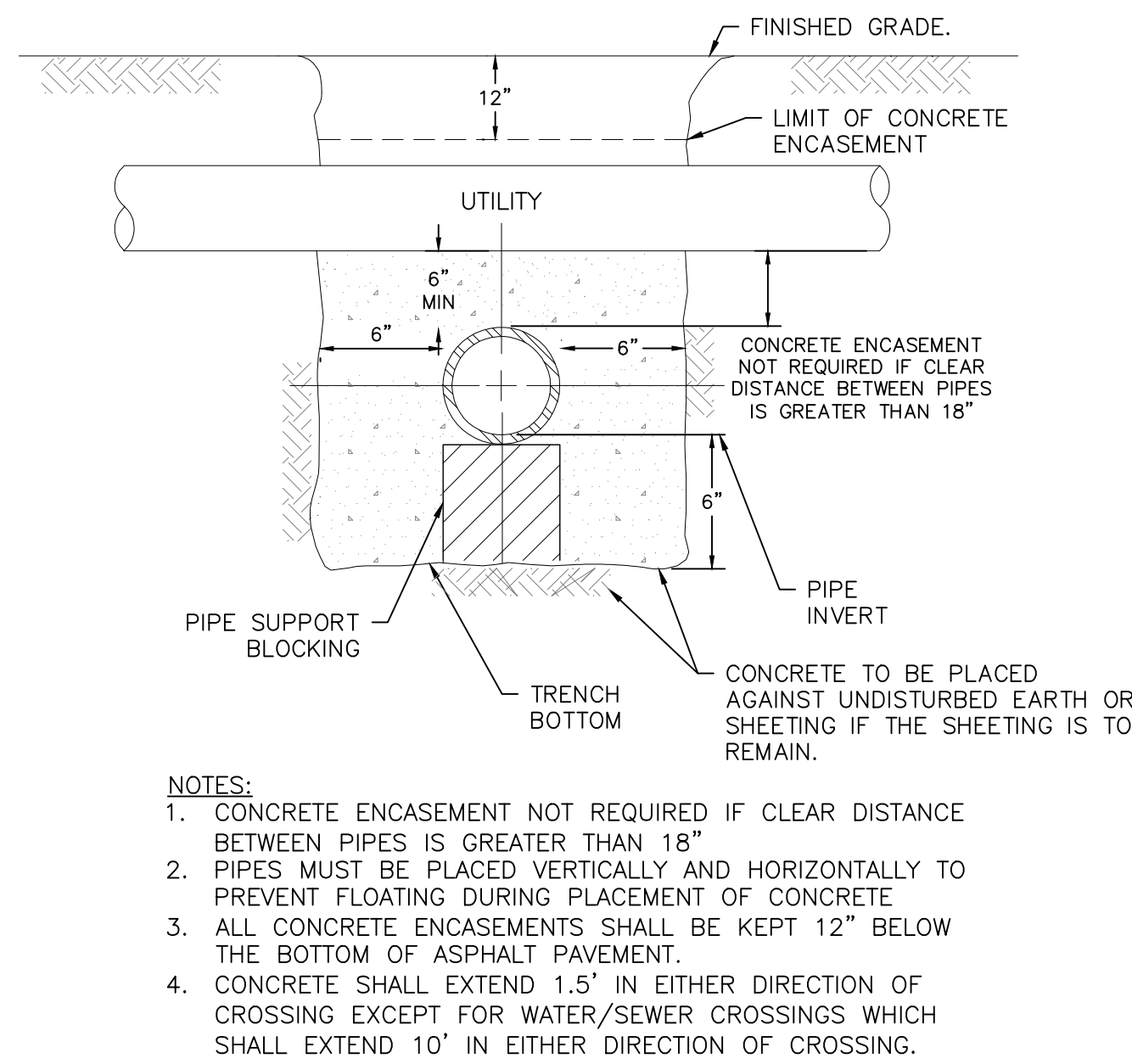
7 BUILDING SEWER

Not to Scale



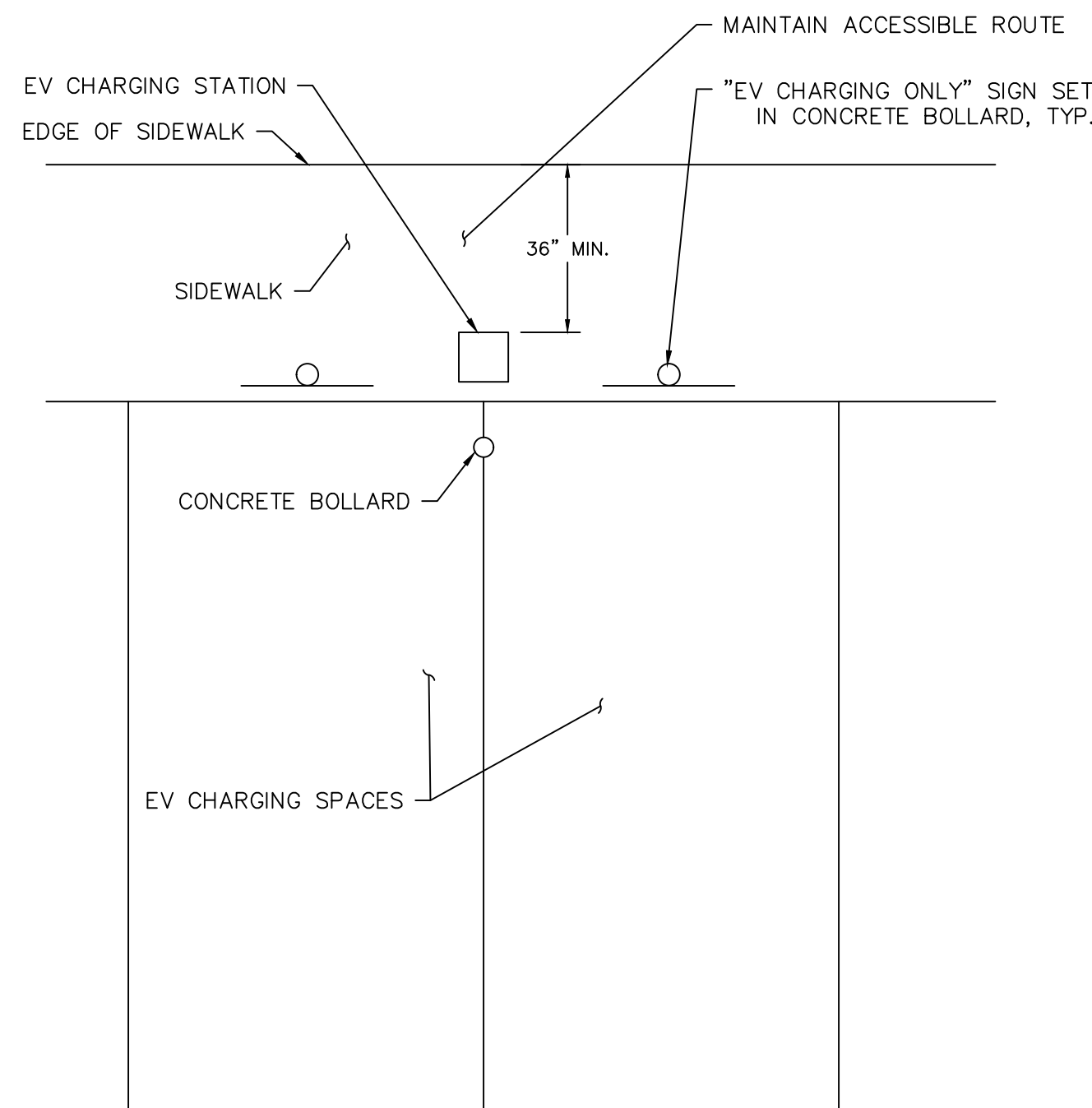
8 CONCRETE BOLLARD

Not to Scale



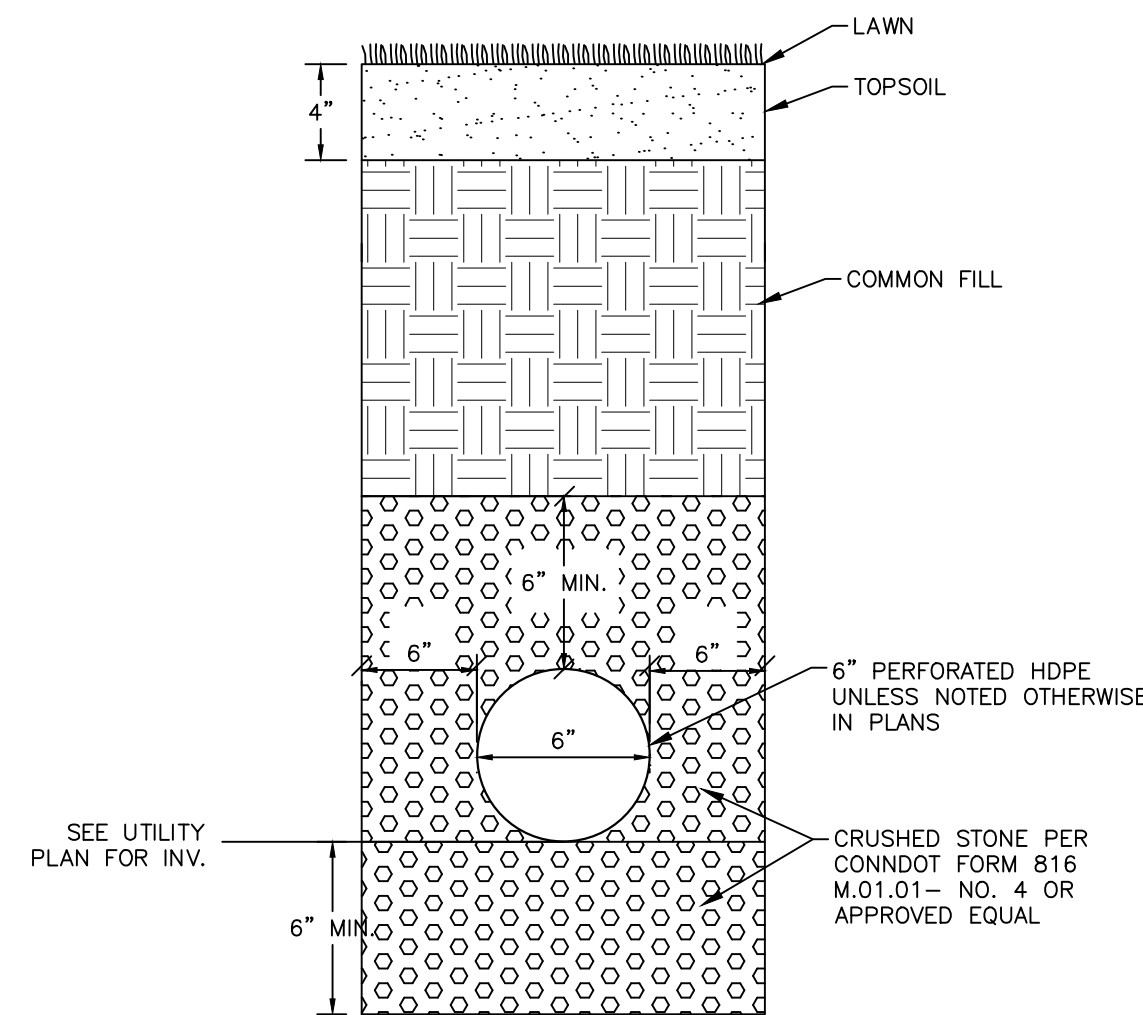
9 STORM & SEWER CROSSING CONCRETE ENCASEMENT

Not to Scale



10 EV CHARGING STATION LAYOUT

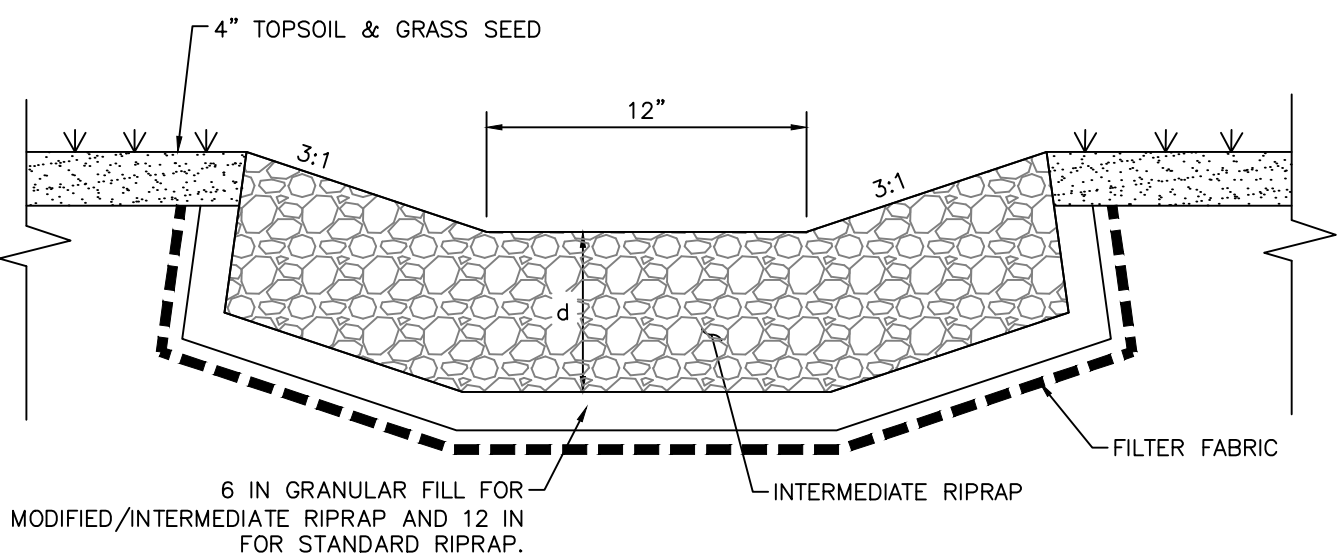
Not to Scale



11 UNDERDRAIN

Not to Scale

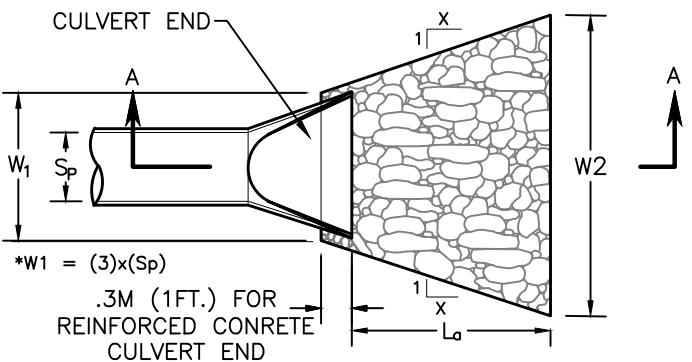
PROPERTY OWNER:
FOUR WOODS LLC
C/O JOHN J WOODCOCK III
89 SADDLEBACK DRIVE
SOUTH WINDSOR, CT 06074
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SCANNELL PROPERTIES #644, LLC
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1 RIPRAP APRON SECTION VIEW

Not to Scale

DETAIL SHOWN PER 2002 CONDOT DRAINAGE MANUAL CHAPTER 11 STORM DRAINAGE SYSTEMS SECTION 13 OUTLET PROTECTION FIGURE 11-13 TYPE A AND B RIP RAP APRON

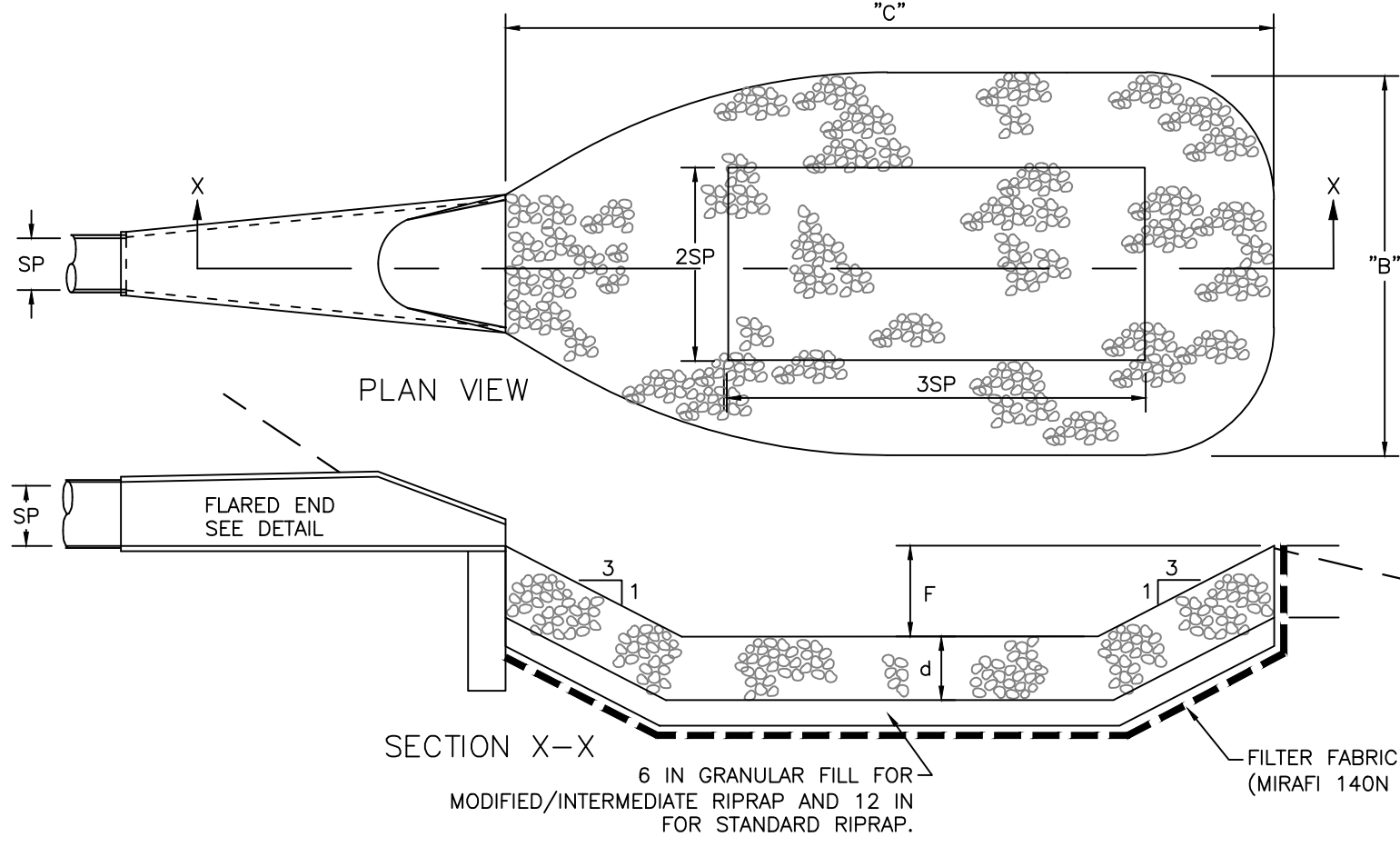


2 RIPRAP APRON PLAN VIEW

Not to Scale

NOTES:

- 1) d = 12 IN (WHEN MODIFIED RIPRAP SPECIFIED)
- 18 IN (WHEN INTERMEDIATE RIPRAP SPECIFIED)
- 36 IN (WHEN STANDARD RIPRAP SPECIFIED)
- 2) RIPRAP GRADATIONS SHALL MEET CT DOT FORM 816 SECTION M.12.02
- 3) FILTER FABRIC SHALL BE NONWOVEN AND SHALL MEET AASHTO M288-00, CLASS 2

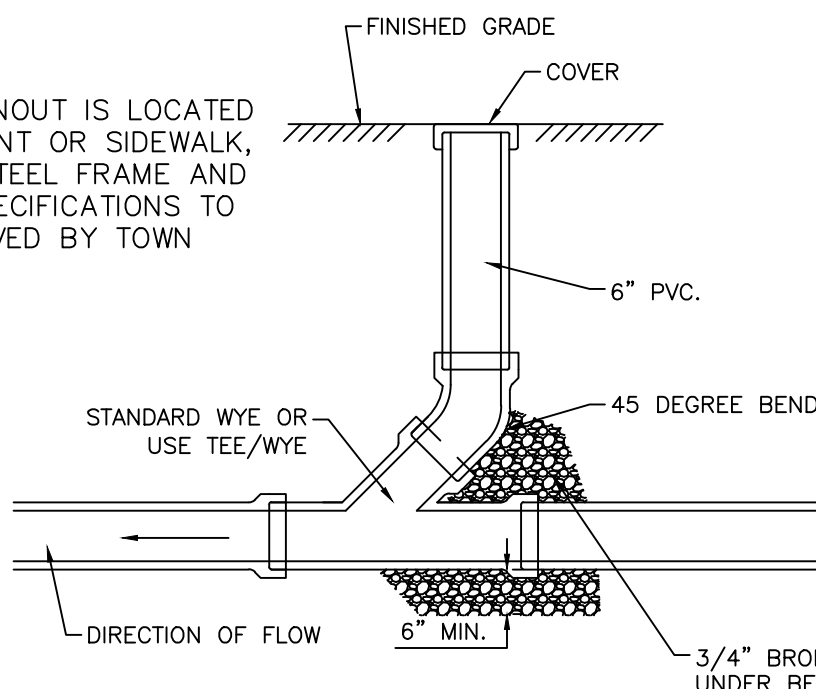


3 RIPRAP PREFORMED SCOUR HOLE

Not to Scale

NOTES:

1. IF CLEANOUT IS LOCATED IN PAVEMENT OR SIDEWALK, PROVIDE STEEL FRAME AND GRATE. SPECIFICATIONS TO BE APPROVED BY TOWN ENGINEER.



4 CLEAN OUT

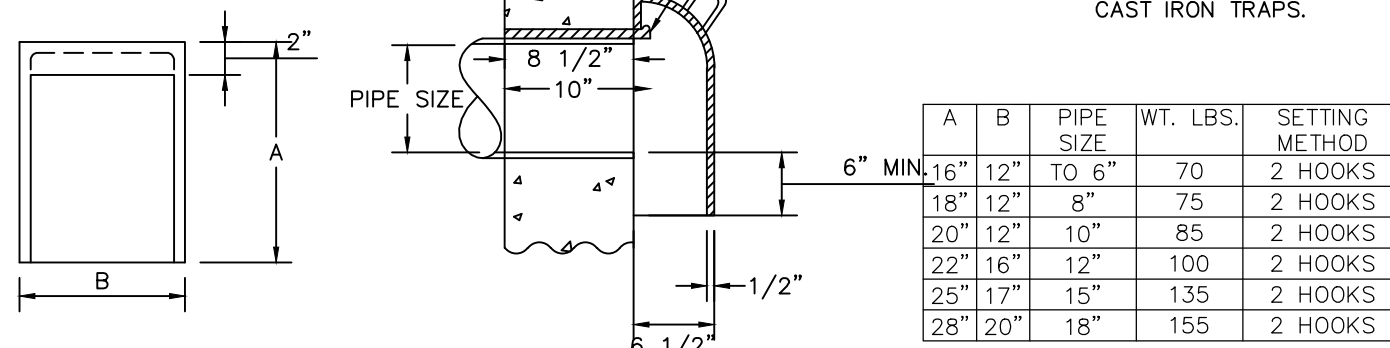
Not to Scale

5 CATCH BASIN TRAP HOOD

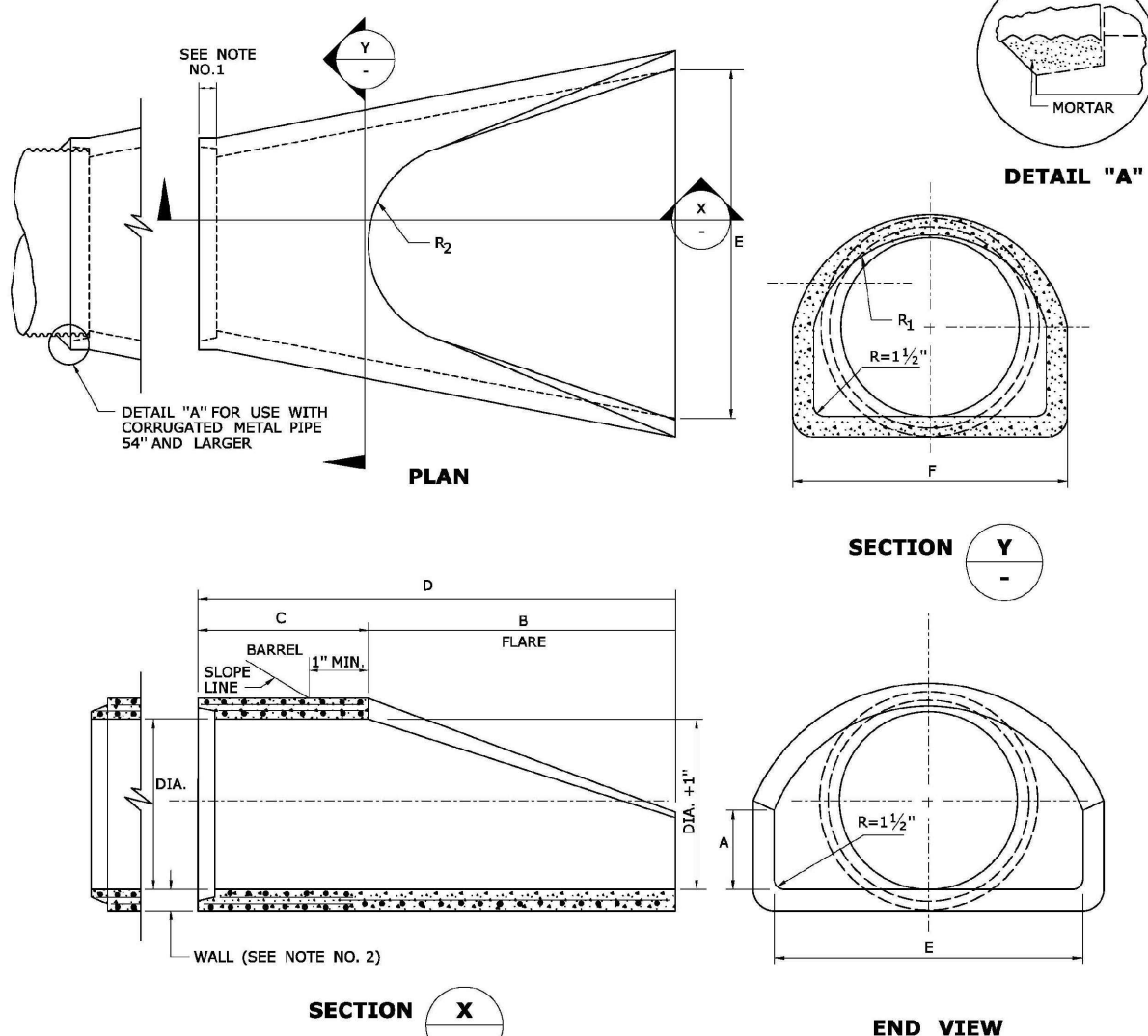
Not to Scale

NOTE:

1. MANUFACTURER CATALOG NUMBER TO BE INDICATED ON SHOP DRAWINGS FOR CAST IRON TRAPS.



A	B	PIPE SIZE	WT. LBS.	SETTING METHOD
16"	12"	TO 6"	70	2 HOOKS
18"	12"	8"	75	2 HOOKS
20"	12"	10"	85	2 HOOKS
22"	16"	12"	100	2 HOOKS
25"	17"	15"	135	2 HOOKS
28"	20"	18"	155	2 HOOKS



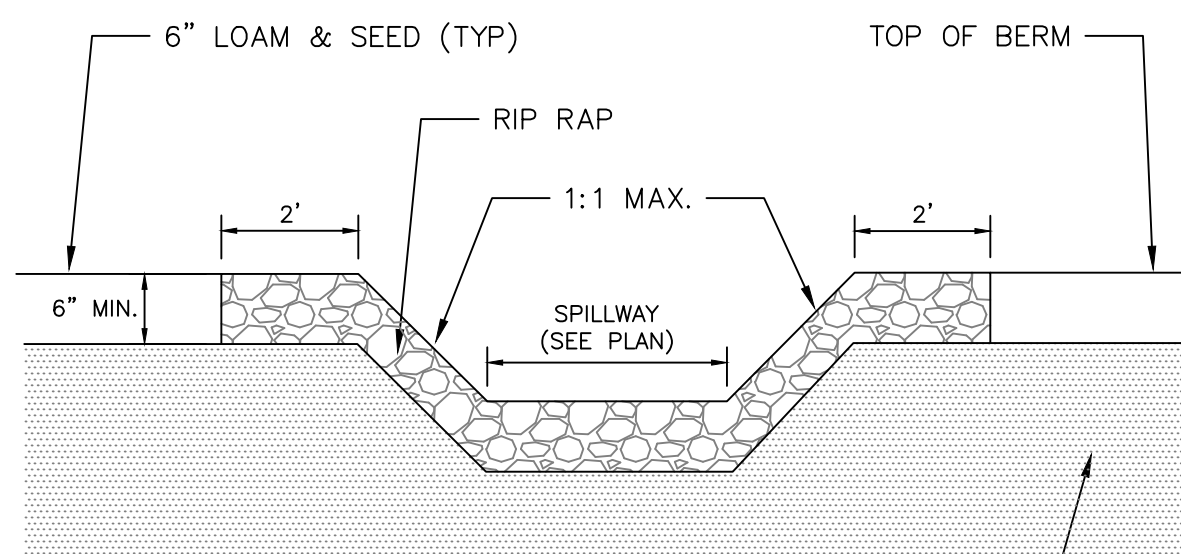
DIA.	A	B	C	D	E	F	R ₁	R ₂	FLARE REINFORCEMENT MIN. AREA OF CONCRETE WALL, MIN. AREA OF TRANSVERSE STEEL, SQ. IN. PER FT.
12"	4"	2'-0"	4'-0 1/2"	6'-0 1/2"	2'-0"	1'-4 1/2"	10 1/2"	9"	0.048
15"	6"	2'-3"	3'-10"	6'-1"	2'-6"	2'-0 1/2"	1'-4 1/2"	1'-1"	0.054
18"	9"	2'-3"	3'-10"	6'-1"	3'-0"	2'-5"	1'-3 1/2"	1'-0"	0.060
21"	9"	2'-11"	3'-2"	6'-1"	3'-0"	2'-7 1/2"	1'-4 1/2"	1'-1"	0.066
24"	9 1/2"	3'-7 1/2"	2'-6"	6'-1 1/2"	4'-0"	2'-8 1/2"	1'-4 1/2"	1'-2"	0.072
30"	1'-0"	4'-6"	1'-7 1/2"	6'-1 1/2"	5'-0"	3'-1"	1'-4 1/2"	1'-3"	0.084
36"	1'-3"	5'-3"	2'-10 1/2"	6'-0"	6'-0"	3'-11 1/2"	2'-0 1/2"	1'-8"	0.096
42"	1'-9"	5'-3"	2'-11"	8'-2"	6'-6"	4'-9 1/2"	2'-3 1/2"	1'-10"	0.108
48"	2'-0"	6'-0"	2'-2"	8'-2"	7'-0"	4'-8 1/2"	2'-4 1/2"	1'-10"	0.120
54"	2'-3"	5'-9"	2'-11"	8'-4"	7'-6"	5'-8 1/2"	2'-4 1/2"	2'-0"	0.132
60"	2'-9"	5'-0"	3'-3"	8'-3"	8'-0"	6'-0 1/2"	3'-0 1/2"	2'-0"	0.144

8 FLARED END TRASH GUARD

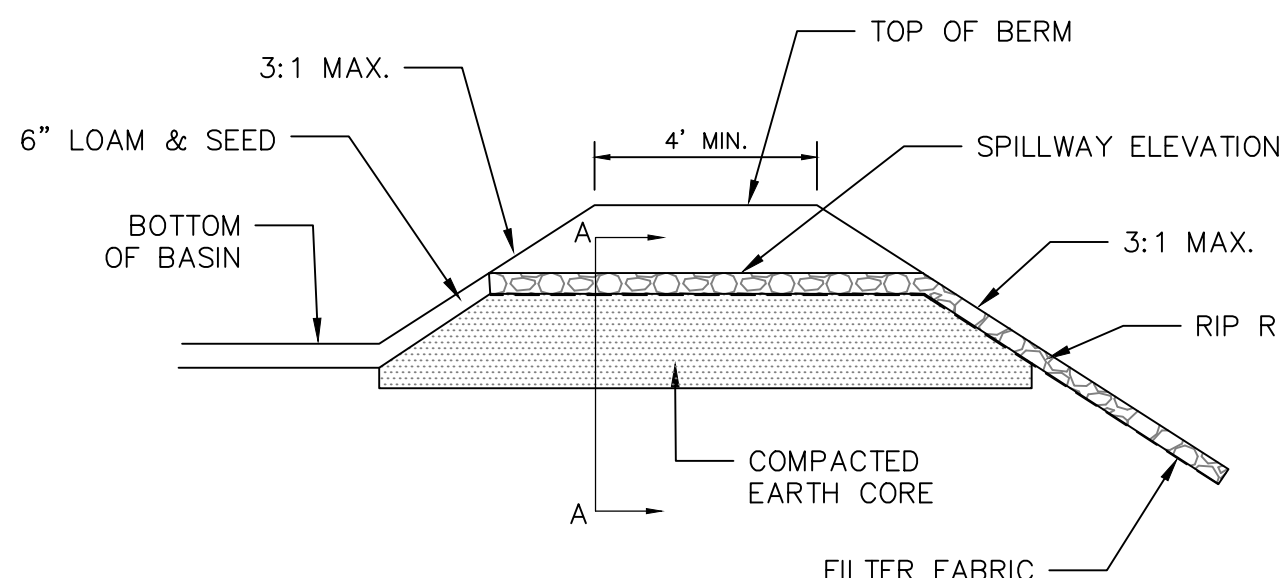
Not to Scale

9 REINFORCED CONCRETE PIPE FLARED END

Not to Scale



SECTION A-A THROUGH SPILLWAY

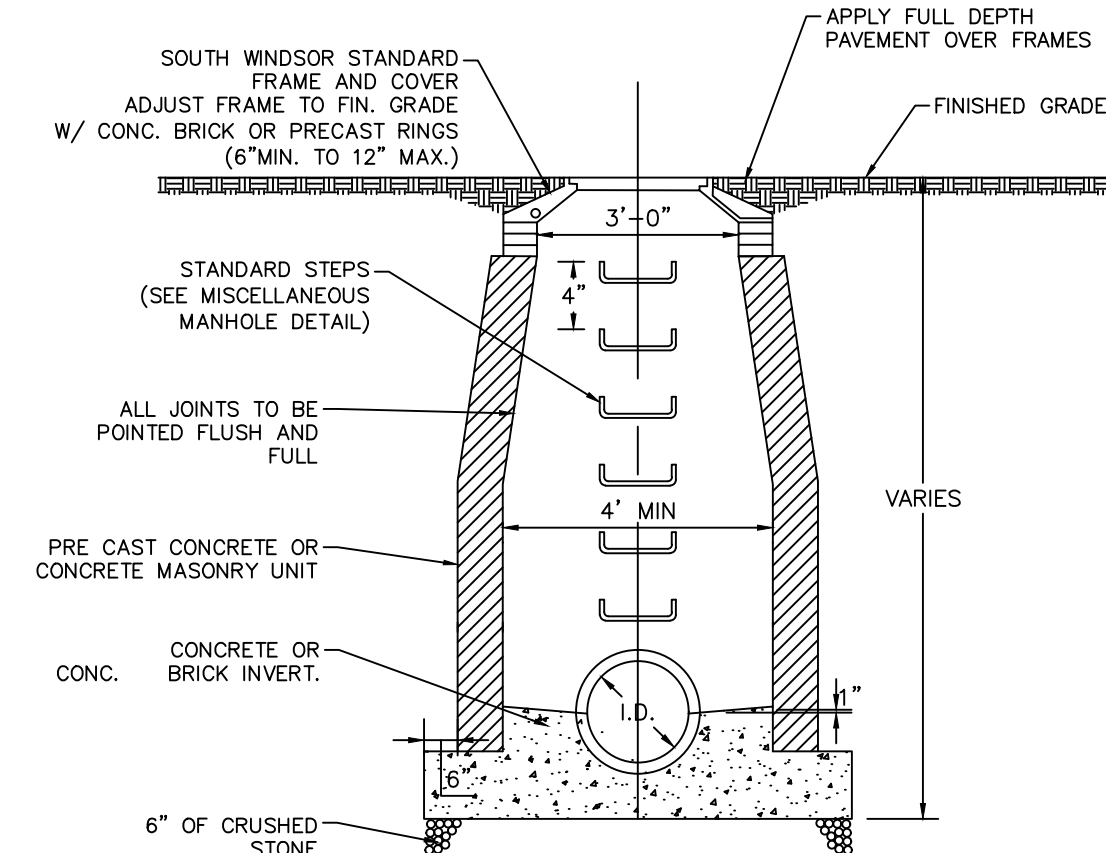


12 EMERGENCY SPILLWAY

Not to Scale

6 TYPE C & CL CATCH BASIN

Not to Scale



NOTES:

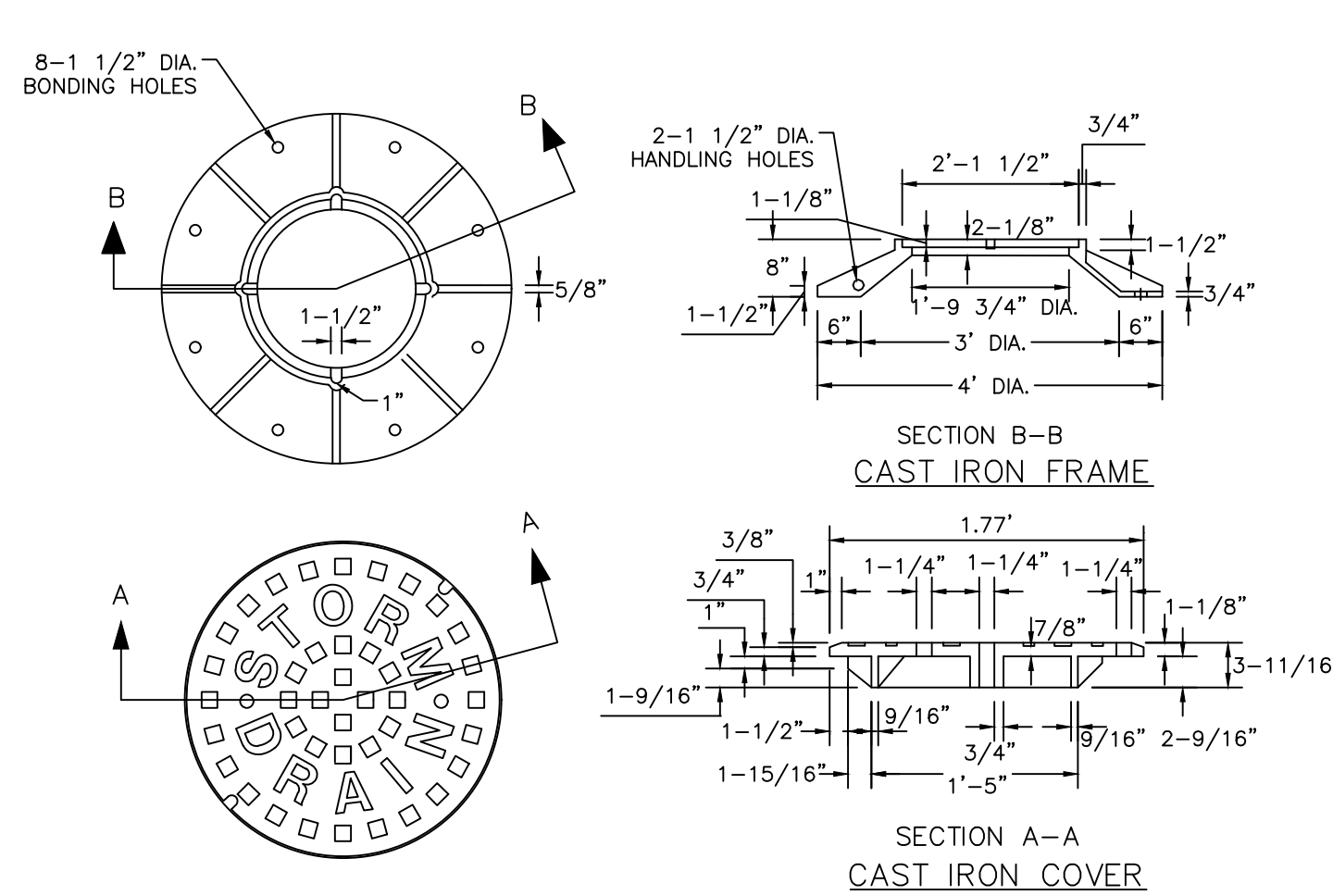
1. REFER TO SHEET C-DR3 FOR NYLOPLAST (PLASTIC) MANHOLE DETAIL.
2. WALL THICKNESS SHALL BE SUFFICIENT TO MEET HS 20 LOADING.
3. WALL THICKNESS FOR STRUCTURES OVER 10' HIGH IS 12" FOR CONCRETE BLOCK UNITS, INSIDE DIMENSIONS REMAIN THE SAME. 3. ALL PIPES SHALL BE CUT FLUSH WITH INSIDE WALLS.

10 STORM DRAIN MANHOLE

Not to Scale

7 TYPE C & CL CATCH BASIN - TYPE II

Not to Scale

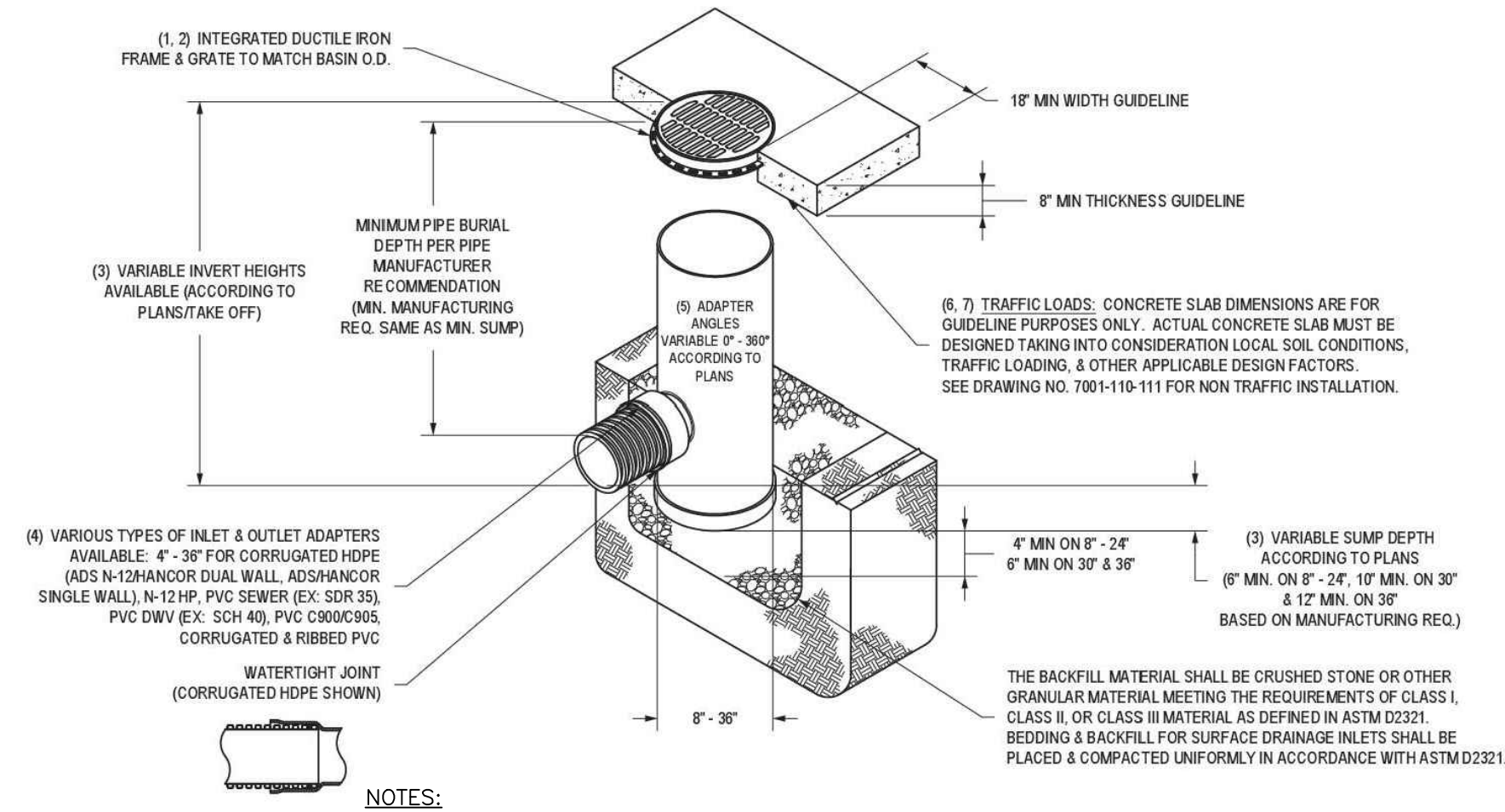
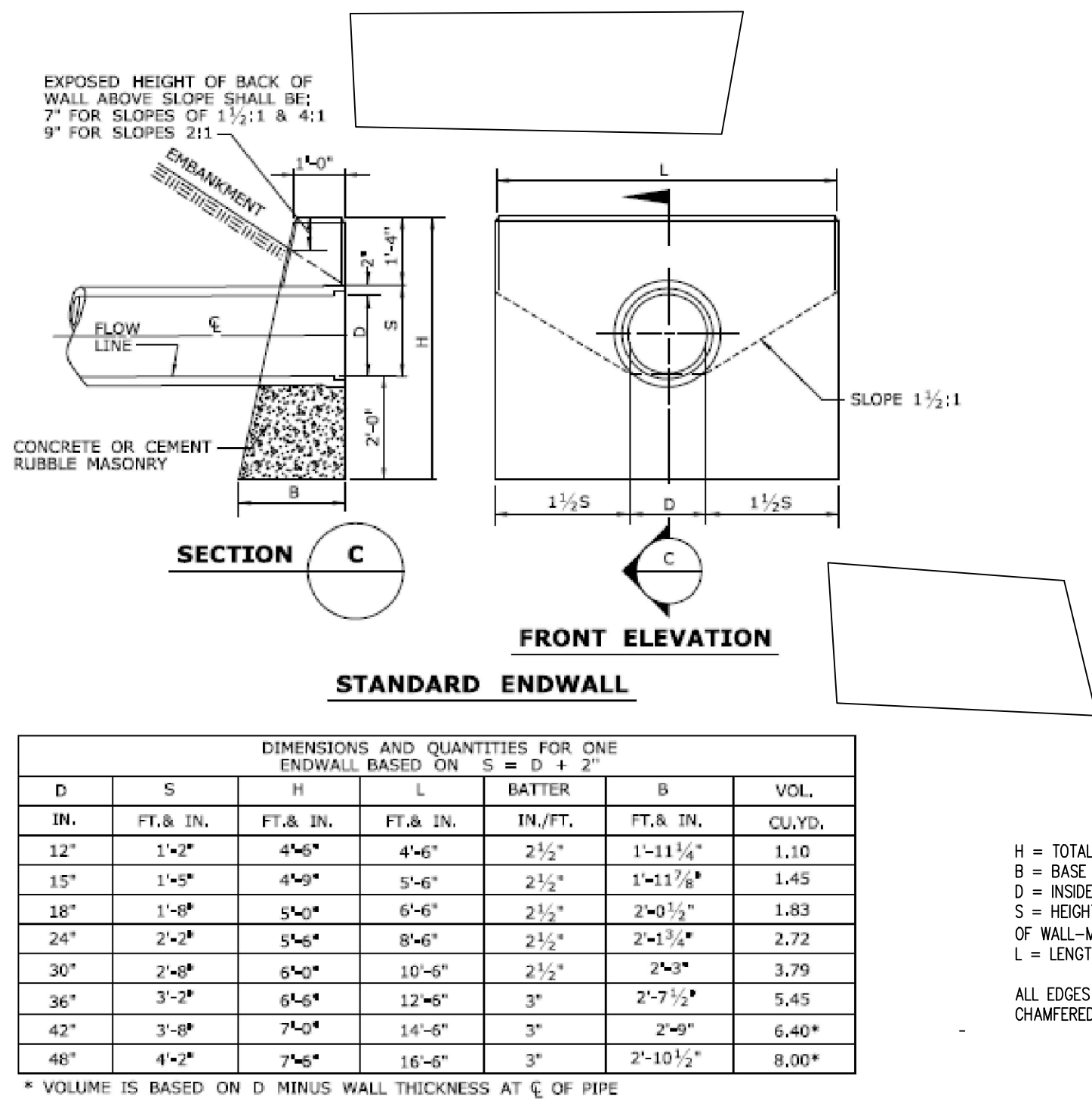
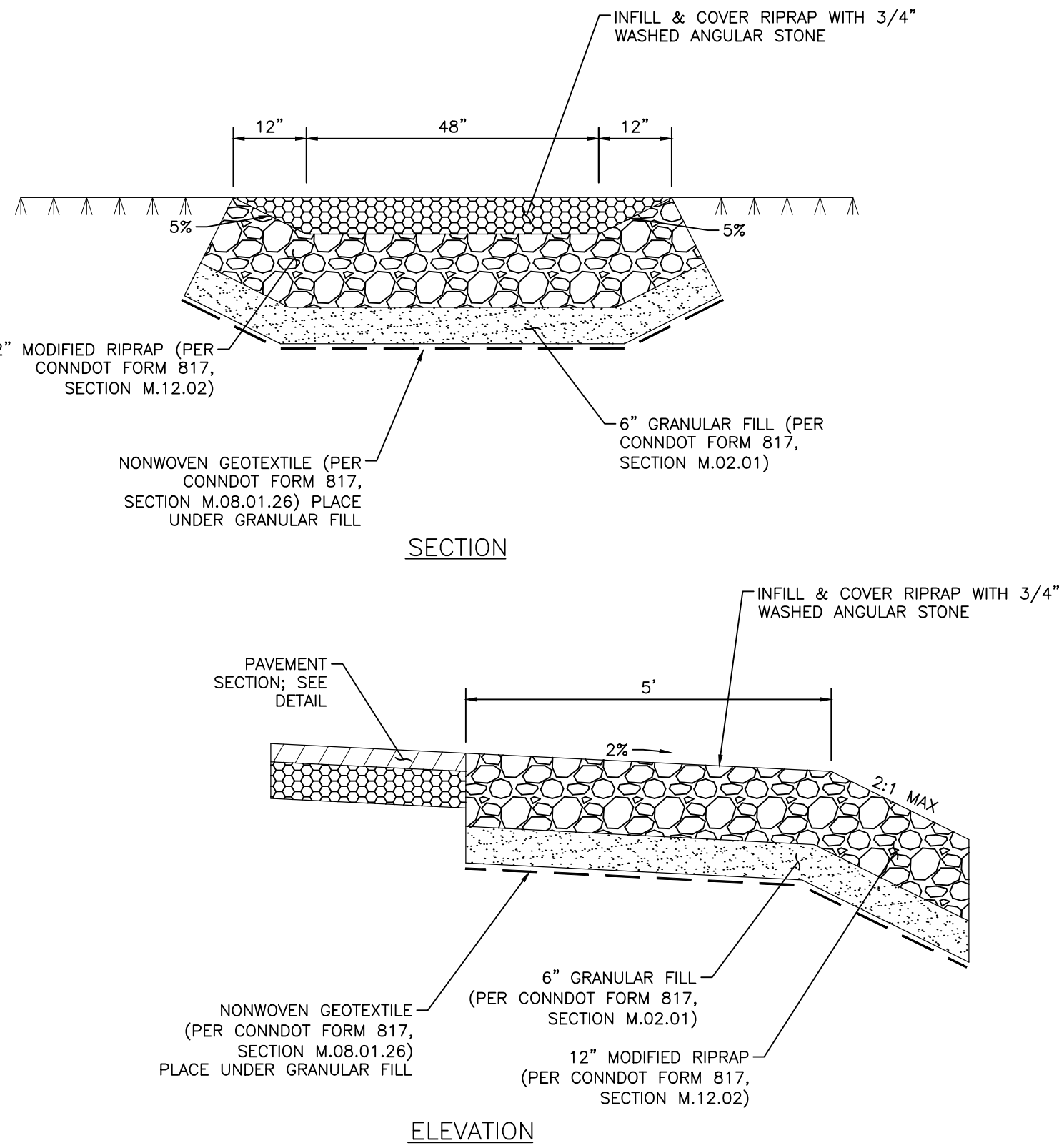


NOTES:

1. THE LOWER SURFACE OF THE COVER AND THE CORRESPONDING UPPER SURFACE OF THE FRAME SHALL BE MACHINE FINISHED TO PROVIDE A SMOOTH FLAT CONTACT OR FIT, WITHOUT ANY TENDENCY FOR THE COVER TO ROCK OR RATTL.
2. SANITARY SEWER MANHOLES SHALL BE EQUIPPED WITH VENT HOLE IN CENTER.

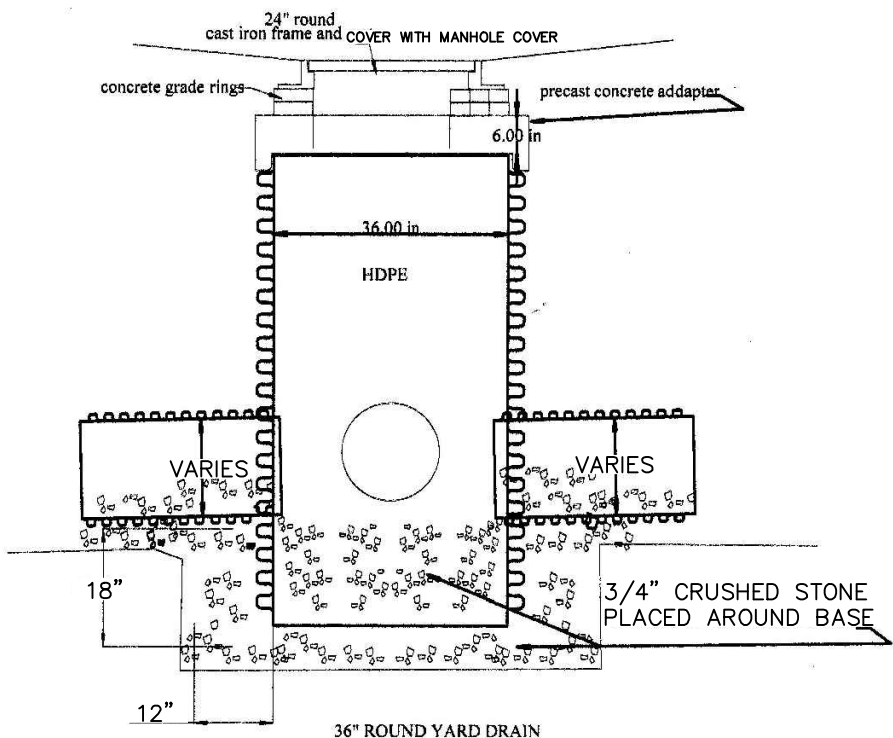
11 MANHOLE FRAME AND COVER

Not to Scale



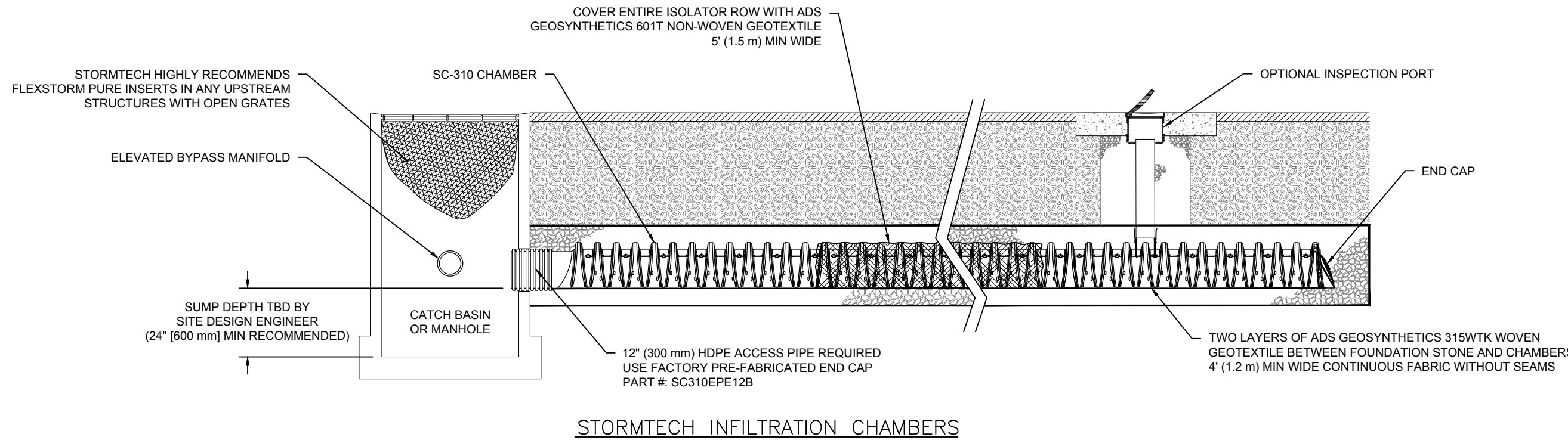
1 PAVED LEAKOFF

Not to Scale



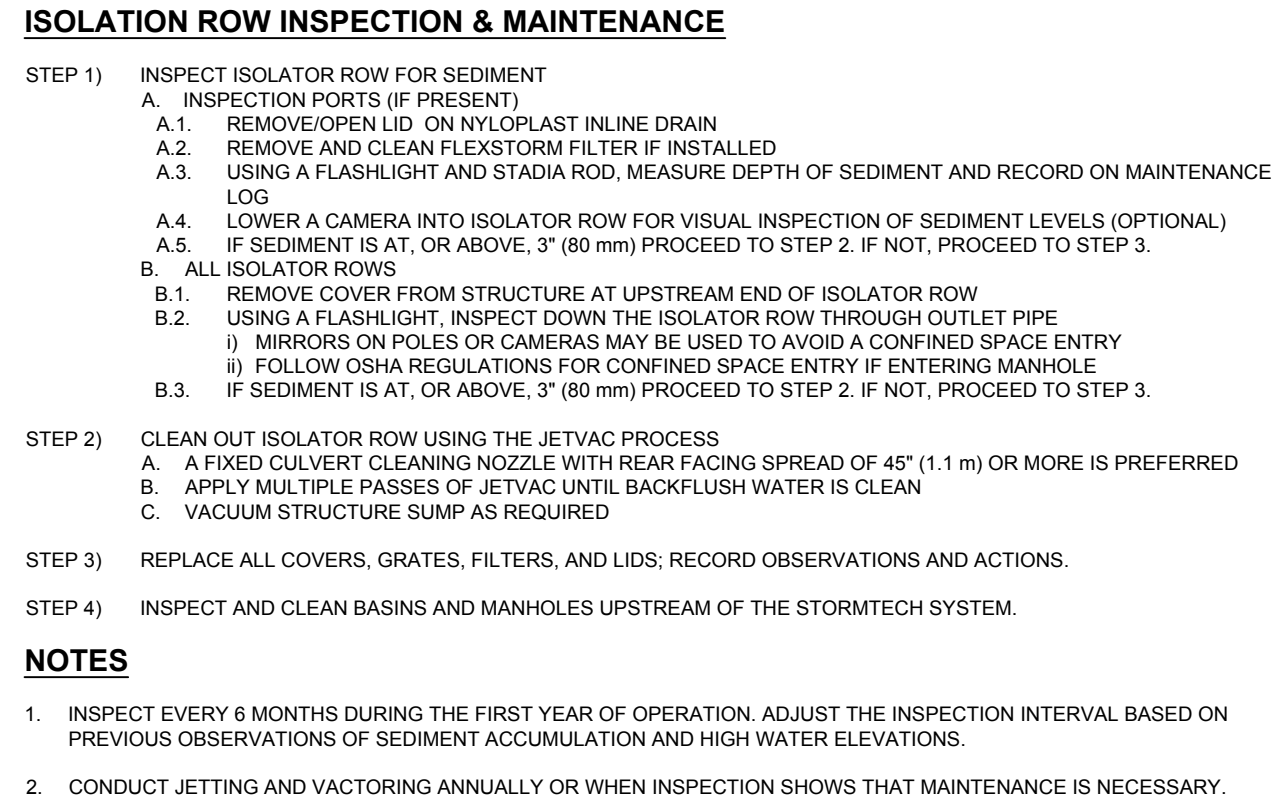
2 STANDARD CONCRETE ENDWALL

Not to Scale



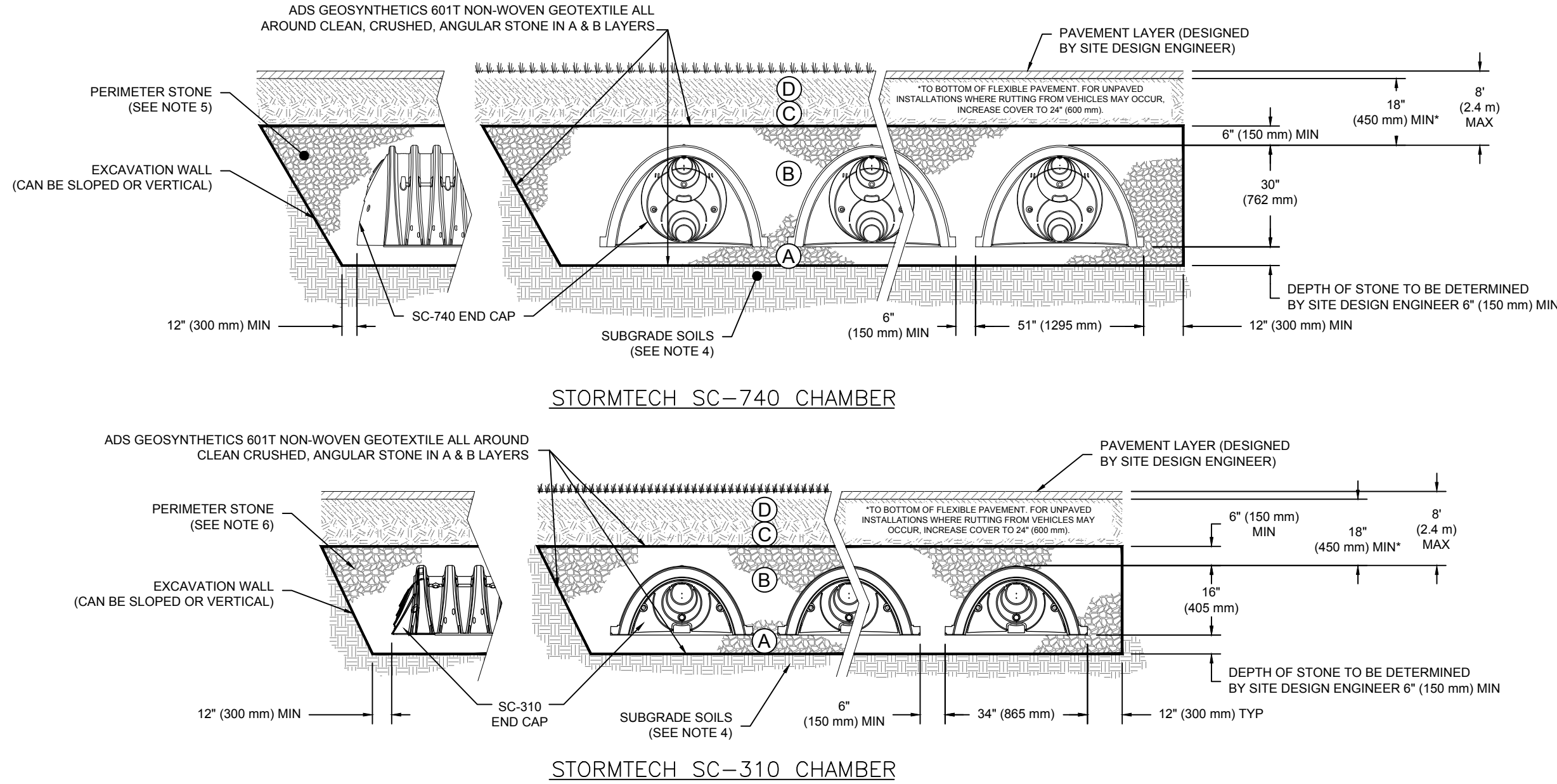
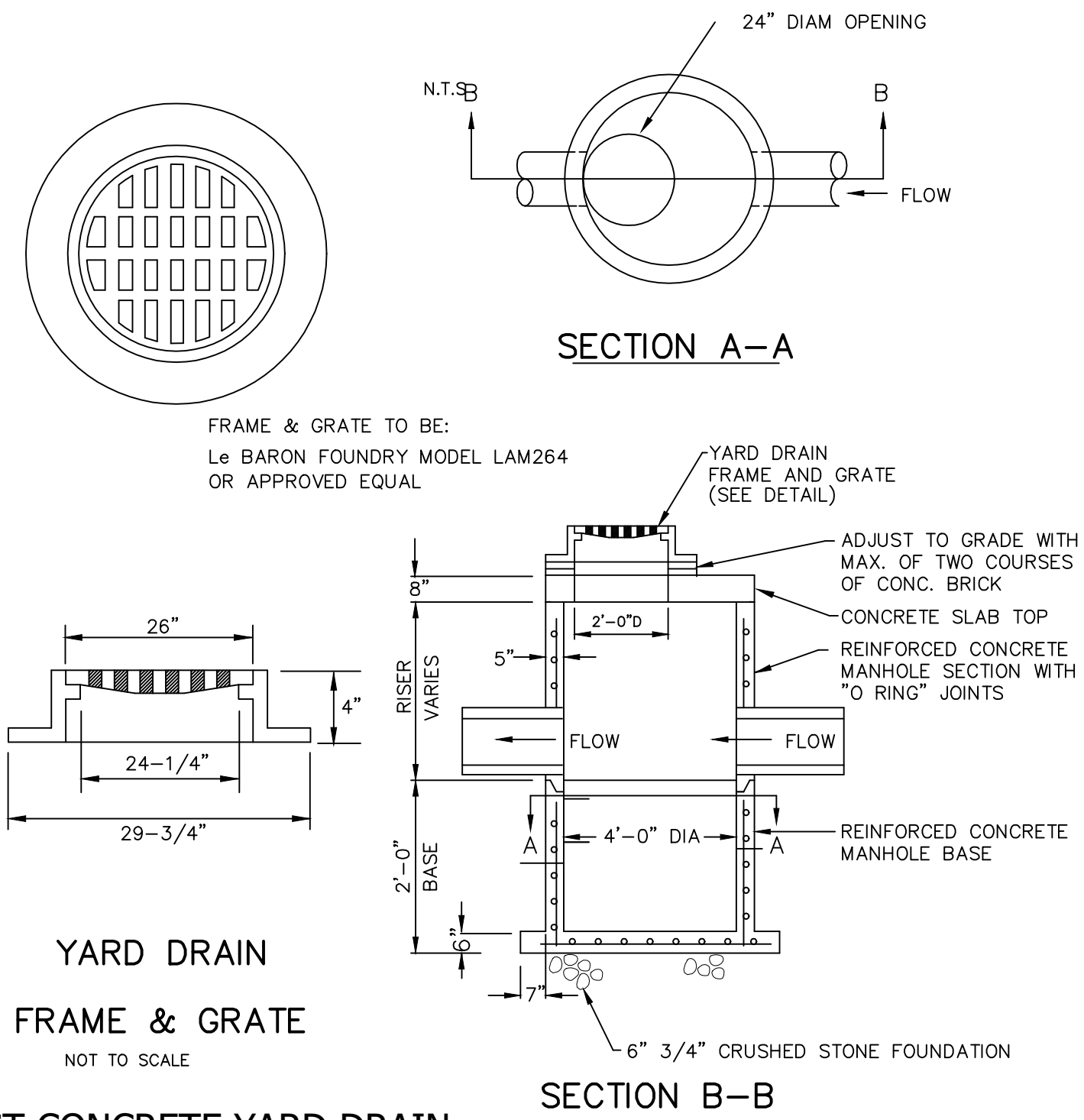
3 PLASTIC YARD DRAIN AND DRAIN MANHOLE (12"-30" DIA.)

Not to Scale



4 HDPE YARD DRAIN

Not to Scale



5 PRECAST CONCRETE YARD DRAIN

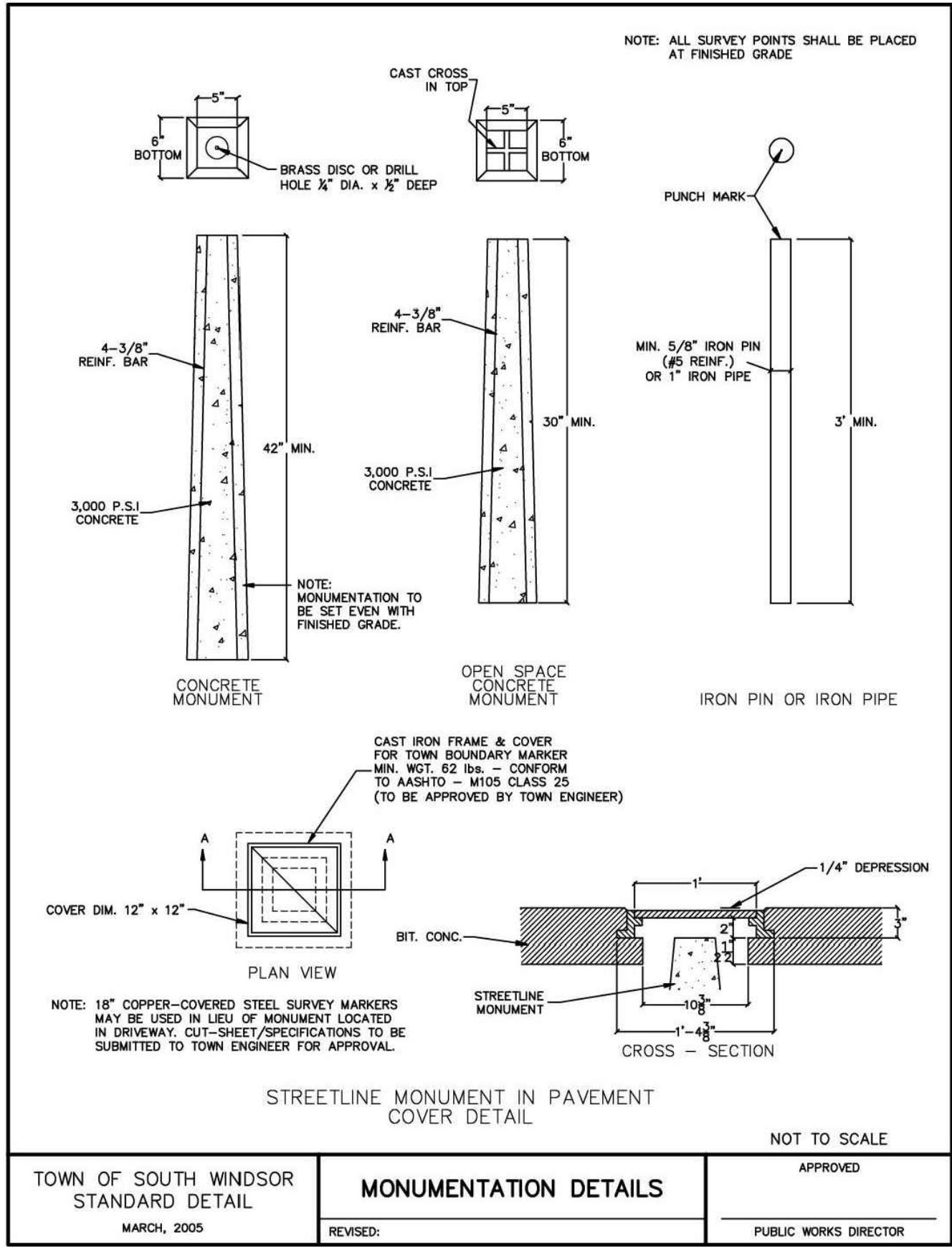
Not to Scale

6 UNDERGROUND STORMWATER CHAMBERS

Not to Scale

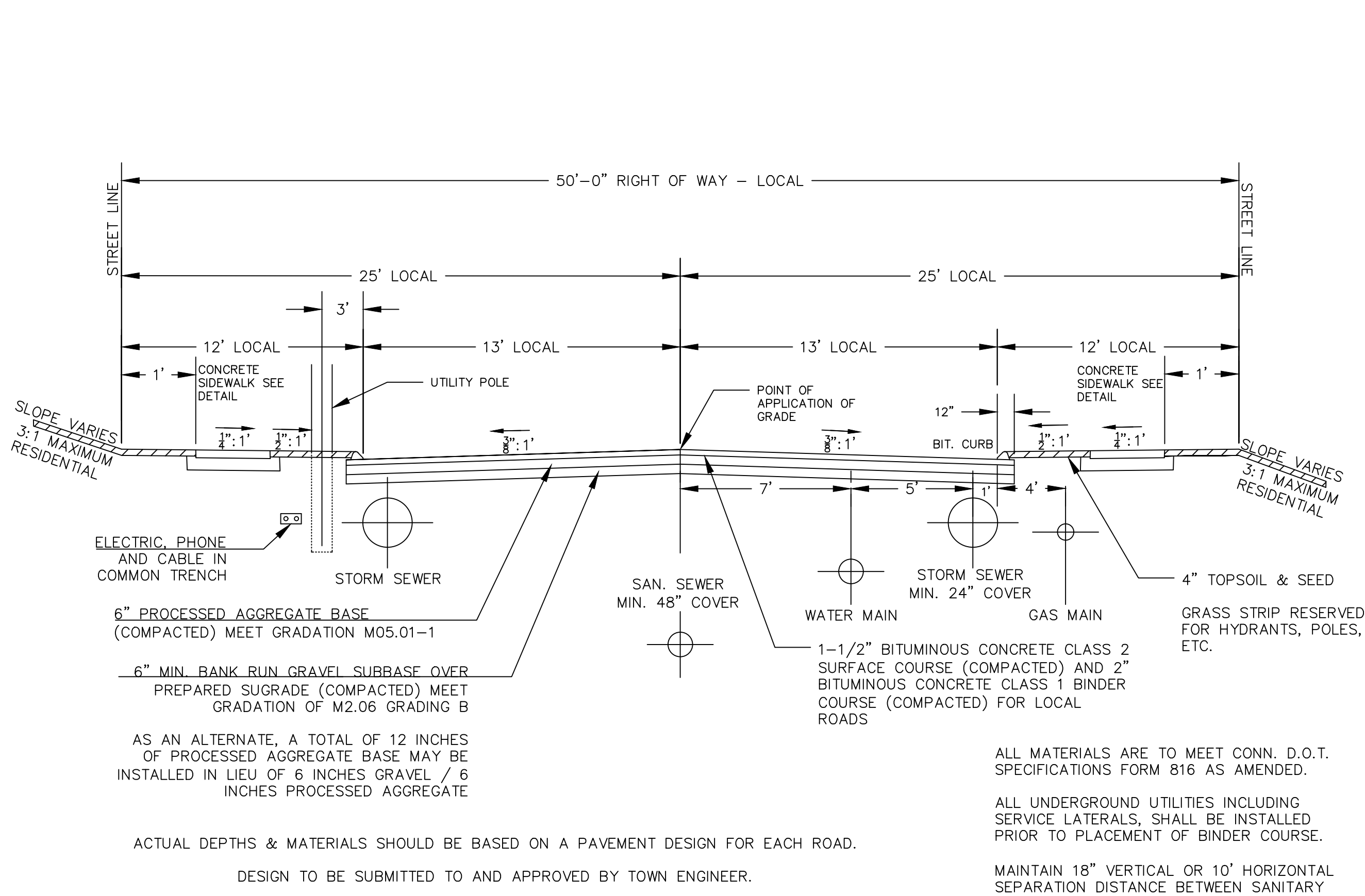
- NOTES:
1. STORMTECH UNDERGROUND CHAMBERS SHOWN FOR ILLUSTRATIVE PURPOSES. CONTRACTOR TO USE THIS PRODUCT OR APPROVED EQUAL.
 2. REFER TO PRODUCT MANUFACTURE SPECS FOR APPROVED MATERIALS AND INSTALLATION INSTRUCTIONS.

PROPERTY OWNER:
FOUR WOODS LLC
C/O JOHN J WOODCOCK III
89 SADDLEBACK DRIVE
SOUTH WINDSOR, CT 06074
APPLICANT:
SCANNELL PROPERTIES #644, LLC
294 GROVE LANE EAST
SUITE 140
WAYZATA, MN 55391
(763) 331-8851



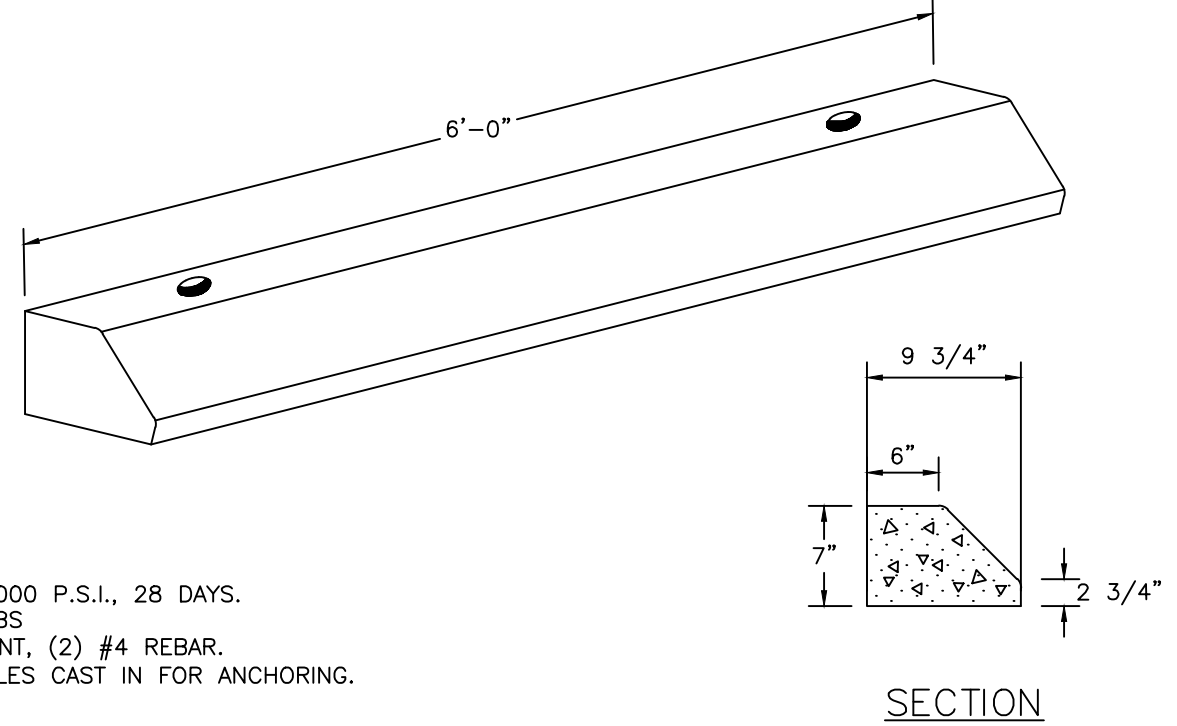
1 TOWN OF SOUTH WINDSOR MONUMENTATION DETAILS

Not to Scale



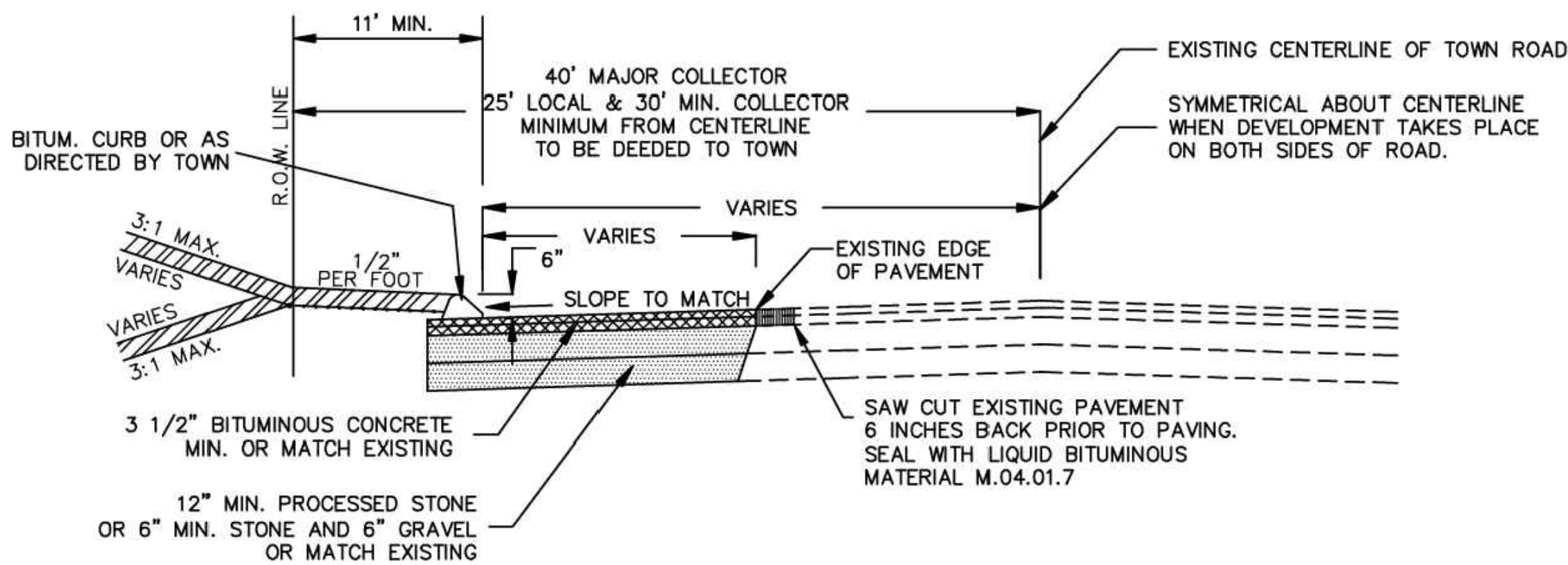
2 TYPICAL ROADWAY SECTION - LOCAL ROADS

Not to Scale



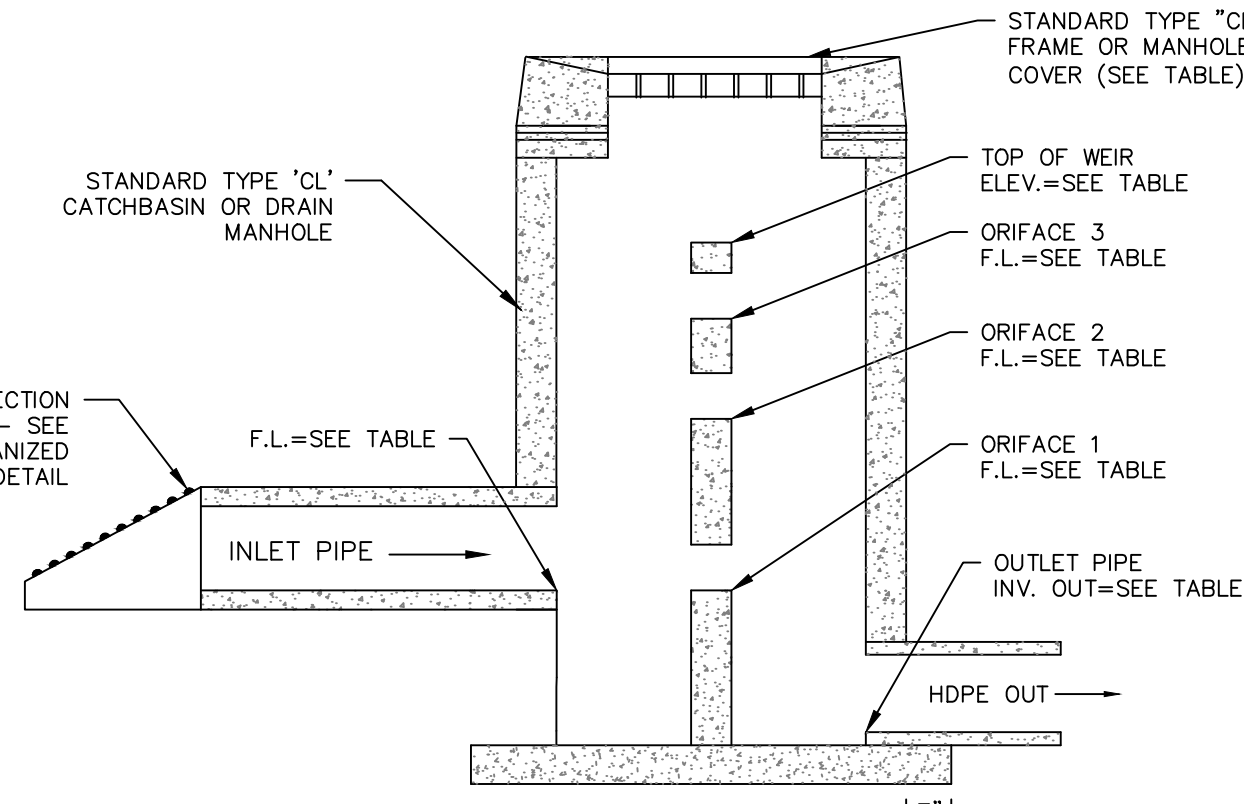
3 PRECAST CONCRETE WHEELSTOP

Not to Scale



4 IMPROVEMENT TO EXISTING TOWN ROADWAY SECTION

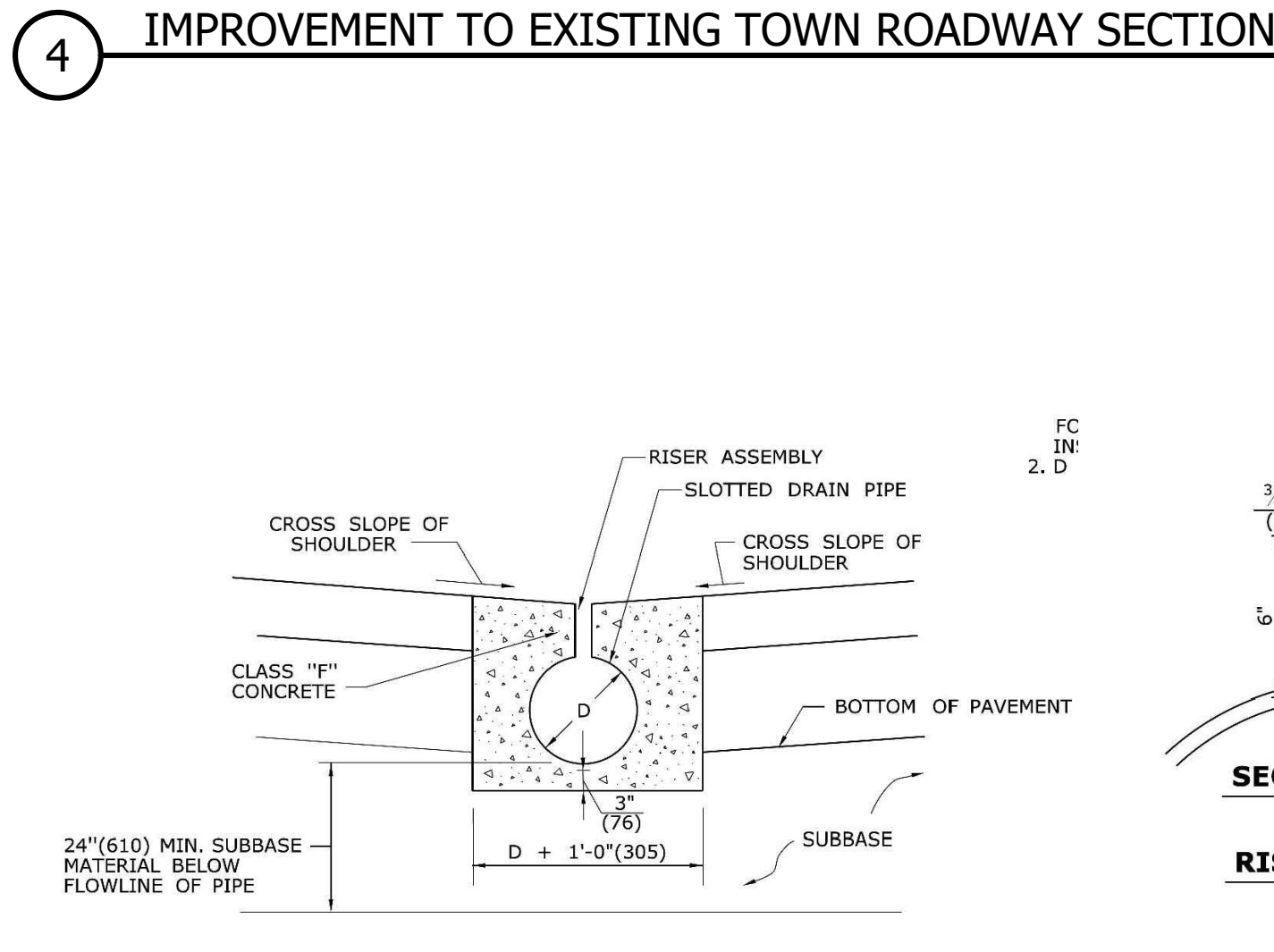
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5 OUTLET CONTROL STRUCTURE

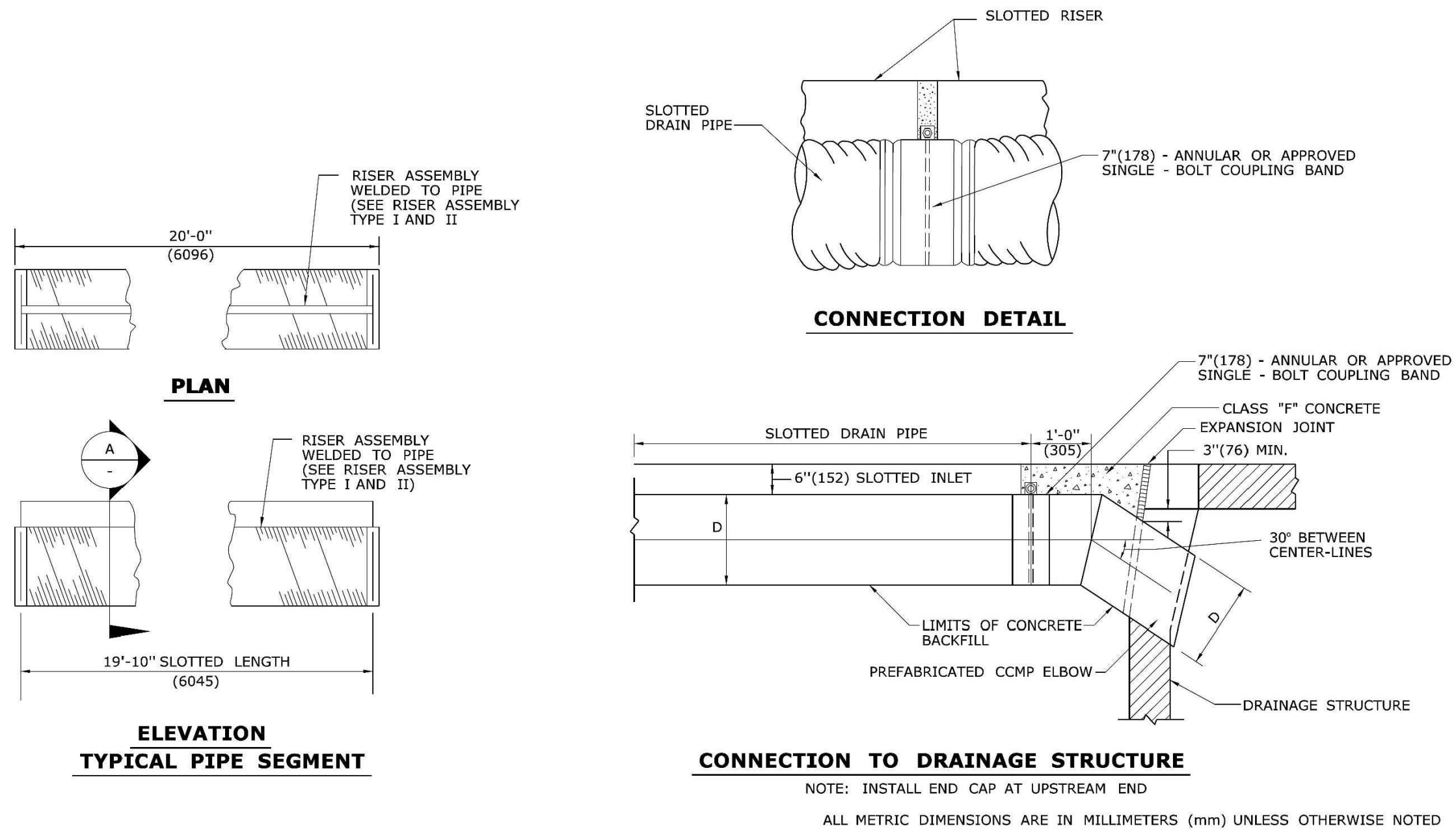
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OUTLET CONTROL STRUCTURE							
OCS NO.	TYPE	INLET INVERT	ORIFICE 1 (ELEV.)	ORIFICE 2 (ELEV.)	ORIFICE 3 (ELEV.)	TOP OF WEIR	OUTLET INVERT
OCS-1	DRAIN MH	80.00 (12")	1"ø (80.00)	1"ø (80.66)	9"x3" (82.16)	82.90	80.00 (12")
OCS-2	DRAIN MH	80.23 (12")	12"ø (80.80)	N/A	N/A	81.90	79.67 (12")
OCS-3	DRAIN MH	83.86 (12")	1"ø (83.88)	1"ø (84.50)	N/A	85.05	83.26 (12")
OCS-4	DRAIN MH (60" ø)	82.78 (12"x3)	9"x2" (82.82)	16"x10" (84.12)	6"x4.5" (85.35)	85.80	82.78 (24")
OCS-5	DRAIN MH	85.80 (12"x2)	N/A	N/A	N/A	N/A	85.80 (15")
OCS-6	"C-L" CB	79.79 (12")	4"ø (80.00)	N/A	N/A	81.95	79.79 (12")
OCS-7	"C-L" CB	83.35 (12")	2"ø (83.40)	N/A	N/A	85.50	83.35 (12")



6 SLOTTED DRAIN PIPE

Not to Scale



PROPERTY OWNER:
FOUR WOODS LLC
C/O JOHN J WOODCOCK III
89 SADDLEBACK DRIVE
SOUTH WINDSOR, CT 06074

APPLICANT:
SCANNELL PROPERTIES #644, LLC
294 GROVE LANE EAST
SUITE 140
WAYZATA, MN 55391
(763) 331-8851

DETAILS

SHEET

C-D6

SHEET 24 OF 24

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21 EBBEY DRIVE
P.O. BOX 167
SOUTH WINDSOR, CT 06074
860-298-8727 - F
www.designprofessionalsinc.com

design Professionals
CIVIL & TRAFFIC ENGINEERS / LAND SURVEYORS
PLANNERS / LANDSCAPE ARCHITECTS

PREPARED FOR:
Scannell Properties #644, LLC
294 Grove Lane East
Suite 140
Wayzata, MN 55391
763-331-8851 T

PROJECT NO.:
4670

DATE:
3/30/2022

DESIGNED BY:
DHL/BPW

CHECKED BY:
DHL/BPW

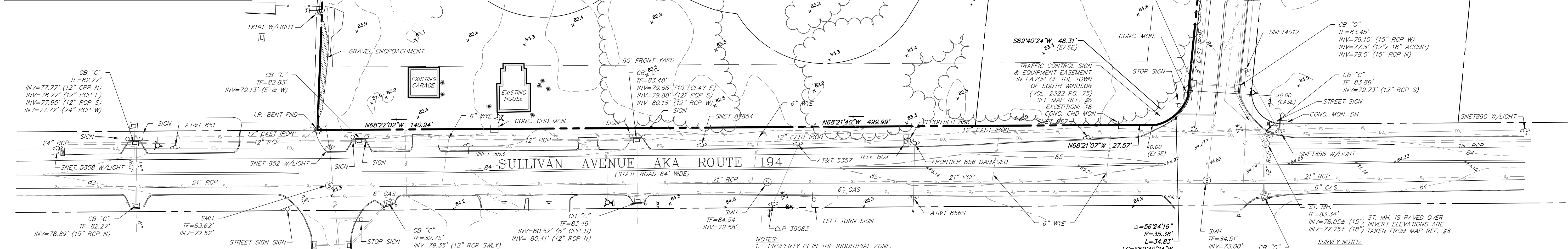
APPROVED BY:
DHL/BPW

**67 KENNEDY ROAD
WAREHOUSE &
DISTRIBUTION CENTER**
352 SULLIVAN AVENUE, 67 & 68 KENNEDY ROAD
SOUTH WINDSOR, CONNECTICUT 06074
GIS NOS: 87300352, 49800067, & 49800068

REVISIONS

NO.	DATE	BY	DESCRIPTION
1	4/19/22	DHL	EAS PLAN UPDATES
2	4/20/22	DHL	P2C SUBMISSION

LEGEND	
EXISTING	DESCRIPTION
BORINGS	
	BORING / TEST PIT LOCATION
COMMUNICATION	
	OVERHEAD COMM. LINES (CABLE, TEL, ETC.)
	APPROX. UNDERGROUND COMMUNICATION LINES
CONTROL POINTS	
	BENCHMARK
DOMESTIC WATER	
	APPROX. WATER MAIN
	APPROX. WATER SERVICE
	WATER VALVE
	FIRE HYDRANT
LIGHTING	
	POLE MOUNTED LIGHT
NATURAL GAS	
	GAS VALVE
	APPROX. GAS MAIN
	APPROX. GAS SERVICE LINE
POWER	
	ELECTRICAL LINES, OVERHEAD
	APPROX. ELECTRICAL LINES, UNDERGROUND
	UTILITY POLE
	UTILITY POLE WITH LIGHT
	UTILITY POLE WITH TRANSFORMER
PROPERTY	
	PROPERTY LINE
	EASEMENT LINE
	IRON PIPE
	IRON ROD
	MONUMENT
ROADS	
	GUARD RAIL
	SIGN
SITE FEATURES	
	EDGE OF WATER
	BARBED WIRE FENCE
	CHAIN LINK FENCE
	RAIL FENCE
	STOCKADE FENCE
	WIRE FENCE
	STONE WALL
	TREE
	TREE LINE
SANITARY SEWER	
	APPROX. SANITARY SEWER MAIN
	APPROX. SANITARY SEWER SERVICE LINE
	SANITARY SEWER MANHOLE
	SEWER CLEAN OUT
STORM SEWER	
	APPROX. STORM DRAIN PIPE
	STORM DRAIN MANHOLE
	CURB INLET
	CATCH BASIN
TOPOGRAPHY	
	CONTOUR
	SPOT ELEVATION
WETLANDS	
	WETLANDS LINE



MAP REFERENCES:

1. STATUTORY DIVISION PLAN ALTA REALTY COMPANY LLC 300 RYE STREET SOUTH WINDSOR, CONNECTICUT DATED: 5-12-11 REV. THROUGH 12-21-12 SCALE: 1"=100' BY: DESIGN PROFESSIONALS, INC.
2. WETLANDS MAP PREPARED FOR JOHN J. WOODCOCK SULLIVAN AVENUE SOUTH WINDSOR, CONNECTICUT DATED: 2-2-05 SCALE: 1"=80' BY: DESIGN PROFESSIONALS, INC.
3. RESUBDIVISION PROPERTY OF MILTON & ELEANOR BAYER LOT 3 AND ALSO PROPERTY OF JOHN WOODCOCK J. LOTS 1, 2, & 4 KENNEDY ROAD SOUTH WINDSOR, CT DATED: 11-12-87 SCALE: 1"=40' BY: LUCEK ENGINEERING ASSOC. INC.
4. PROPERTY OF MARTIN F. MCGRATH, ET AL. SOUTH WINDSOR, CONN. SCALE: 1"=100' DATED 8-30-56 REV. 11-25-56 BY: CECIL W. BROOKS
5. LOCATION OF RIGHT OF WAY OF THE NORTHERN CONNECTICUT POWER COMPANY ACROSS THE PROPERTY OF AHERN BROS. INC. TOWN OF SOUTH WINDSOR SCALE: 1"=200' DATED: FEB. 1933
6. LOCATION OF RIGHT OF WAY OF THE NORTHERN CONNECTICUT POWER COMPANY ACROSS THE PROPERTY OF THOMAS AND BRIDGET CONNERS TOWN OF SOUTH WINDSOR SCALE: 1"=200' DATED: FEB. 1933
7. BOUNDARY & EASEMENT PLAN PREPARED FOR MCHANE CONSTRUCTION COMPANY & THE TOWN OF SOUTH WINDSOR A PORTION OF LAND OF FOUR WOODS, LLC BY: J.R. RUSSO & ASSOC., DATED: 1-25-13
8. CONNECTICUT STATE HIGHWAY DEPARTMENT RIGHT OF WAY MAP TOWN OF SOUTH WINDSOR EAST WINDSOR HILL - WAPPING ROAD FROM THE HARTFORD-SPRINGFIELD ROAD SOUTHEASTERLY ABOUT 12,500 FEET ROUTE NO. 194 SCALE 1"=40' DATED 8-31-33 NUMBER 132-02 SHT NO. 2 OF 4
9. AS-BUILT PLAN FEDEX GROUND 40 KENNEDY ROAD SOUTH WINDSOR, CT BY: J.R. RUSSO & ASSOC., LLC DATED: 11-15-12 REV. THROUGH 10-12-15 SCALE: 1"=40'

NOTES:

1. PROPERTY IS IN THE INDUSTRIAL ZONE.
2. #67 KENNEDY ROAD & 352 SULLIVAN AVENUE CONTAIN 740,055 SQUARE FEET OR 16.989 ACRES.
3. #68 KENNEDY ROAD CONTAINS 105,480 SQUARE FEET OR 2.421 ACRES
4. HORIZONTAL DATUM IS BASED ON NAVD83. VERTICAL DATUM IS BASED ON NAVD88
5. BUILDINGS ON BOTH PARCELS ARE EXISTING NON-CONFORMING TO FRONT YARD SETBACKS.
6. PROPERTY DOES NOT FALL WITHIN THE LIMITS OF A SPECIAL FLOOD HAZARD ZONE AS DEPICTED ON: "FIRM FLOOD INSURANCE RATE MAP NUMBER 09003C0377" TOWN OF SOUTH WINDSOR CONNECTICUT EFFECTIVE DATE: 9-26-2008 FEDERAL EMERGENCY MANAGEMENT AGENCY FEDERAL INSURANCE ADMINISTRATION.
7. UNDERGROUND UTILITY, STRUCTURE AND FACILITY LOCATIONS DEPICTED AND NOTED HEREON HAVE BEEN COMPILED, IN PART, FROM RECORD MAPPING SUPPLIED BY THE RESPECTIVE UTILITY COMPANIES OR GOVERNMENTAL AGENCIES, FROM PAROL TESTIMONY AND FROM OTHER SOURCES. THESE LOCATIONS MUST BE CONSIDERED AS APPROXIMATE IN NATURE. ADDITIONALLY, OTHER SUCH FEATURES MAY EXIST ON THE SITE, THE EXISTENCE OF WHICH ARE UNKNOWN TO DESIGN PROFESSIONALS, INC. THE SIZE, LOCATION AND EXISTENCE OF ALL SUCH FEATURES MUST BE FIELD DETERMINED AND VERIFIED BY THE APPROPRIATE AUTHORITIES PRIOR TO CONSTRUCTION.
8. CONTRACTOR SHALL CONTACT "CALL BEFORE YOU DIG" FOR UNDERGROUND UTILITY MARKING AT LEAST TWO FULL WORKING DAYS PRIOR TO START OF CONSTRUCTION: 1-800-922-4455 OR WWW.CBYD.COM.
9. FENCE ALONG WESTERLY PROPERTY LINE ON LAND OF JKLM, LLC IS IN POOR CONDITION. THE LINE DEPICTED HEREON IS ALONG THE BASE OF THE EXISTING FENCE. THE TOP OF THE FENCE LEANS WESTERLY 1-4 FEET ONTO LAND OF FOUR WOODS LLC.
10. TOPOGRAPHIC INFORMATION FOR THIS PROJECT WAS PROVIDED BY GOLDEN AERIAL SURVEYS BASED ON THE STATE OF CONNECTICUT AERIAL IMAGERY, OBTAINED IN 2016. THE TOPOGRAPHIC INFORMATION FOR SULLIVAN AVENUE & KENNEDY ROAD WAS OBTAINED BY DESIGN PROFESSIONALS, INC. BY CONVENTIONAL SURVEY TECHNIQUES.
11. INLAND WETLANDS DEPICTED HEREON WERE FIELD DELINEATED BY ALL-POINTS TECHNOLOGY CORPORATION ON 8/17/21 AND WERE FIELD LOCATED BY DESIGN PROFESSIONALS, INC.

SURVEY NOTES:

THIS SURVEY AND MAP HAS BEEN PREPARED PURSUANT TO THE REGULATIONS OF CONNECTICUT STATE AGENCIES SECTIONS 20-300b-1 THRU 20-300b-20 AND THE "STANDARDS SUGGESTED METHODS AND PROCEDURES FOR SURVEYS AND MAPS IN THE STATE OF CONNECTICUT" AS ADOPTED BY THE CONNECTICUT ASSOCIATION OF LAND SURVEYORS, INC. ON AUGUST 29, 2019.

- TYPE OF SURVEY IS A PROPERTY & TOPOGRAPHIC SURVEY AND IS INTENDED TO DEPICT THE LOCATION OF EXISTING CONDITIONS RELATIVE TO PROPERTY LINES.
- THIS IS AN INDEPENDENT RESURVEY BASED ON THE REFERENCED MAPS & DEEDS.
- HORIZONTAL ACCURACY MEETS CLASS A-2 STANDARDS. VERTICAL ACCURACY MEETS CLASS V-2 STANDARDS. TOPOGRAPHICAL ACCURACY MEETS CLASS T-2 STANDARDS WITH THE PAID AREAS OF SULLIVAN AND KENNEDY ROADS AND T-3 STANDARDS FOR THE REMAINING SITE.

TO MY KNOWLEDGE AND BELIEF, THIS MAP IS SUBSTANTIALLY CORRECT AS NOTED HEREON.

BARRY D. CLARKE, L.S.

16766
LIC. NO.

21 JEFFREY DRIVE
P.O. BOX 1167
SOUTH WINDSOR, CT 06074
860-291-9757 - F
www.designprofessionalsinc.com

design professionals
CIVIL & TRAFFIC ENGINEERS / PLANNERS / SURVEYORS
GIS ANALYSTS / LANDSCAPE ARCHITECTS

PREPARED FOR:
DANIEL MADRICAL
SCANNELL PROPERTIES
294 GROVE LANE EAST
SUITE 140
WAYZATA, MN 55391

PROJECT NO.
4670

DATE
9-20-21

DESIGN BY
BDC

DRAWN BY
BDC

CHECKED BY
BDC

IN CHARGE
LEG

NO. DATE BY

1	2-9-22	BDC
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REVISIONS
ADD #68 KENNEDY ROAD

PROPERTY & TOPOGRAPHIC SURVEY

SHEET
V-1

