

Town of South Windsor - Proposed zoning regulation changes adopted 5-9-23; effective 5-28-23

Remove Section 1.5 Moratorium on New Truck and Freight Terminals and Warehouses and Distribution Centers to Article 1 Introductions/Districts

Section 2.11 Commercial and Industrial Storage and Display

- A.** No storage of any material outside a building is allowed if such storage is visible from a public way. **See Section 6.2.5 for screening requirements**
- B.** Outdoor storage is prohibited within required yards abutting a required buffer area.
- C.** Limited outdoor display of items for wholesale/retail sale **may be permitted on a site plan** approved by the Commission, provided that such sales are in accordance with Section 4.5.3 Accessory Uses. **The area of display is limited to 20% of the front yard setback and cannot extend along more than 50% of the site's frontage. Any equipment with booms or extensions shall be displayed in a retracted position to minimize visible impact of the equipment. Non-compliance may result in zoning enforcement.**
- D.** See Section 4.2 Buckland Road Gateway Development Zone for storage and display requirements for the Gateway Development Zone; see paragraph 4.4.5.F Site Appearance Requirements for storage and display requirements for the I-291 Corridor Zone. See Section 4.6.3 for storage and display requirements and prohibitions in the Route 5 Travel Services Zone.

Section 2.16 Location and Screening of Ancillary Structures

Trash storage units, dumpsters, air-conditioning units and similar devices shall observe required yard setbacks. Rooftop units shall be screened from the public way. **See Screening requirements Section 6.2.5.**

ARTICLE 4 COMMERCIAL AND INDUSTRIAL ZONES

Section 4.1 Commercial and Industrial Use Regulations

4.1.1 Permitted Uses

Uses within Commercial and Industrial Zones shall be governed by Table 4.1.1A. See Section 4.2 for use provisions relating to the Buckland Road Gateway Development Zone (GD); Section 4.3 for the Designed Commercial Zone (DC); and Section 4.4 for the I-291 Corridor Development Zone (CD).

Table 4.1.1A Permitted Commercial and Industrial Uses

Use	Zones						Additional Provisions
	DC	GC	I	RC	RO	TS	
Residential		SP/SE*	SP/SE*	SP/SE*			A single residential dwelling by the owner of the facility is allowed in conjunction with a permitted commercial use. The residence may be either a dwelling unit contained within the commercial facility itself, or may be a separate free-standing single-family dwelling that otherwise meets all of the requirements of the underlying zone. * An owner can request either the business/residential use be a rental property for properties developed prior to 2019 provided the owner lives on site or operates the business. No more than 10% of properties within 1,000 feet can contain such rentals.
Assisted Living	SE	SE		SE	SE		See Article 7 Special Regulations
Duplex		SE					See Article 7 Special Regulations
Horses and ponies for personal use		SP		SP	SP		See Article 7 Special Regulations
Multi-family residential		SE					See Section 5.10 – Specific Requirements for a Sullivan Avenue Mixed-Use Development in the GC
Public and Semipublic							
Hospitals, Sanitariums, Convalescent Homes		SE					Except for contagious, mental, alcoholic, and drug cases provided that: The lot area is not less than 1/10 acre for each person accommodated, including patients and employees; Such uses are served by the public sewer and water facilities; and 50% impervious coverage
Fraternal organizations and membership clubs operated as a nonprofit activity			SE				
Municipal Facilities and Uses	SE	SE	SE	SE	SE		

Table 4.1.1A Permitted Commercial and Industrial Uses

Use	Zones						Additional Provisions
	DC = Design Commercial GC = General Commercial I = Industrial RC = Restricted Commercial RO = Restricted Office TS = Route 5 Travel Services						
	P = Permitted SE =Special Exception SP =Site Plan ZP = Zoning Permit Blank =Not Permitted						
Use	DC	GC	I	RC	RO	TS	
Commercial Uses							
Agriculture (e.g. forestry operation, cultivated lands, animals)	P	P	P	P	P	P	Clearing of ½ acre of land requires PZC approval.
Adult-Oriented Businesses		SE					See Article 7 Special Regulations
Alcoholic Liquor Stores	SP	SP		SP			
Alcoholic beverage sales for consumption on the premises of hotels, restaurants, taverns, grills and cafes	SP	SP	SE*	SP			*Industrial zone – accessory to a permitted recreational use or manufacturing use
Alcoholic liquor sales as part of a drug store or grocery store	SP	SP		SP			
Amusement - theaters, moving picture houses, assembly halls, billiard and pool rooms, bowling alleys, and similar amusement enterprises		SP	SE*				Excluding shooting galleries, freak shows, mechanical rides and similar enterprises * Industrial zone in conjunction with permitted indoor recreational use. See Article 7 Special Regulations
AUTOMOTIVE USES:							
Automobile retail sales, new and used		SE	SE*				* Maximum cars offered for sale on site is four (4)
Automobile service and repair			SP/SE*				* SE approval if use abuts a residential zone; Commission may determine hours of operation
Automobile service stations, fuel and limited repair		SE					See Article 7 Special Regulations
Automotive fuel	SP/SE						Only when accessory to a retail sales and inventory directly related thereto facility that has a minimum gross floor area of 50,000 sq ft; SE approval if use abuts a residential zone. Commission may determine hours of operation
Automotive vehicle refueling / re-energizing stations						SP	
Brewery / Distillery			SP				Accessory uses and incidental sales (e.g. tasting rooms) subject to criteria in Section 4.5.3 Accessory Uses. Parking is subject to PZC approval
Beverage Stores, quick service; e.g. coffee shop		SP		SP		SP	
Building and landscape materials sales and storage including lumber yards and construction equipment			SE				Excluding the storage or fabricating of structural steel and heavy concrete products. Primary building shall be minimum 2,000 sq ft (excluding greenhouses) except plant materials, outdoor storage/sales area shall not be visible from a public street
Convenience Stores		SP		SP		SP	

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P = Permitted SE = Special Exception SP = Site Plan ZP = Zoning Permit Blank = Not Permitted	DC = Design Commercial GC = General Commercial I = Industrial RC = Restricted Commercial RO = Restricted Office TS = Route 5 Travel Services						
Car/Truck Wash Facility		SP	SP*			SP	*Allowed only as an accessory use in industrial zone
Convention Facilities	SE						
Day Care Facilities	SE	SE	SE	SE	SE		See Article 7 Special Regulations
Dog Grooming Facilities		SE	SE	SE			See Article 7 Special Regulations for Dog Grooming Facilities in the Industrial Zone
Dry cleaning, laundry, and dyeing establishments		SP					
Earth product screening, rock crushing, top soil screening and/or similar type operations			SE				Only permitted as an ACCESSORY USE to a permitted primary use; operations shall be a minimum of 1,000 feet from a residential zone; shall only be allowed during normal business hours (i.e. 8 am – 5 pm) and is subject to the Noise Ordinance
Entertainment (live) at hotels and restaurants, taverns, grills and cafes	SP	SP	SE*				*Industrial zone in conjunction with permitted indoor recreational use. See Article 7 Special Regulations
Equipment sales, service and rentals, including farm equipment		SP	SE				
(EVSE) AC Level 1, AC Level 2, and DC Fast Charging Electric Vehicle Charging Equipment	SE	SE	SE	SE			As a primary use (destination charging) for sale of EV Charging services to the public, similar in intent to internal combustion engine fueling (e.g. gasoline or diesel); allowed as an accessory use in all zones See Appendix H – Electric Vehicle Supply
Financial Services, institutions and agencies	SP	SP	SP	SP		SP	
Freezer lockers and incidental processing of food for human consumption		SP					
Garages - public		SP		SE			
Hotels (and motels)	SP	SP		SP		SP	
Hotels, exclusive of entertainment		SP		SP			

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P = Permitted SE = Special Exception SP = Site Plan ZP = Zoning Permit Blank = Not Permitted	DC = Design Commercial GC = General Commercial I = Industrial RC = Restricted Commercial RO = Restricted Office TS = Route 5 Travel Services						
Manufacture of bricks, cement products, tile and terra cotta			SE				
Manufacture, processing, packaging and assembly of components or goods			SP/SE*				*Facilities over 40,000 sf adjacent to residential zones permitted by Special Exception approval
Microbrewery, Brewpub		SP		SE			No more than 15,000 barrels manufactured per year; wholesale and retail sales allowed; tasting rooms allowed; outside tasting allowed in conformance with Section 7.15 Outdoor Dining. Hours of operation may be determined by the PZC
Mobile Food Vendor	ZP	ZP	ZP	ZP	ZP		Mobile food vendors are permitted in these zones without a permit provided that all of the following criteria are met: 5. 1. Written permission from the property owner; 2. Adequate room for vehicles to pull off safely; 3. A maximum of 12 sq. ft. of free-standing signage; 4. Vendors shall not be located within public rights of way; 5. All facilities and equipment used by vendor must be portable and must be re-moved from the site by 9:00 p.m. 6. Connections to external utilities are not permitted; 7. Vendors must comply with health and safety regulations of the Town of South Windsor and the State of Connecticut. 8. Vendors shall keep the area of operation free of debris and shall clean the area thoroughly upon ceasing operations each day. 10. At least 1 trash container must be provided for use by patrons in conjunction with mobile food vendor.
Mortuaries, funeral and internment services		SP		SP			May have living quarters containing a dwelling unit of at least 700 square feet of living space and at least 2 rooms, exclusive of the bathroom, to be occupied by a person, together with his family, who is the owner, manager, caretaker, or janitor, residing in the same building
Multi-family residential		SE					See Section 5.10 – Specific Requirements for a Sullivan Avenue Mixed-Use Development in the GC zone
Motor vehicle refueling / re-energizing stations						SP	
Offices - professional, commercial, corporate and business	SP	SP	SP	SP	SP	SP	
Personal Services Shops	SP	SP		SP			Limited to barber shops, beauty shops, shoe repair shops, tailoring and dressmaking shops, tanning salons, clothes rental stores, and similar establishments. Tattoo parlors are not considered a personal service shop.

Table 4.1.1A Permitted Commercial and Industrial Uses

Use	Zones						Additional Provisions
	DC	GC	I	RC	RO	TS	
Pharmacy	SP	SP		SP		SP	Including pharmacy with drive thru
Petroleum Product Bulk Storage			SE				
Plumbing, heating, electrical, mechanical industrial and general contracting establishments			SP				May include showrooms, storage and maintenance of heavy construction equipment
Printing and publishing, graphic arts processes, sign shop painting		SP	SP				
Radio and television studios and transmitters, communication towers, multi media stations		SP					
Recreational Activities – indoor, including fitness facilities	SP	SP	SE*			SP	*See Article 7 Special Regulations
Recreational Facilities – outdoor		SE	SE*				*See Article 7 Special Regulations
Research Laboratories			SP				
Restaurants, including fast food	SP	SP/SE*		SP/SE*		SP	*Restaurants proposed with drive thru facilities permitted by Special Exception
Restaurants, exclusive of entertainment		SP		SP			
Retail Establishments with high turnover (frequent customer arrivals / departures)						SP	
Retail sales and inventory directly related thereto	SP	SP		SP			<p>Retail sales and high-turnover uses with frequent customer arrivals and departures are prohibited.</p> <p>Retail sales associated with a club membership format are permitted and shall not be considered a wholesale sales use.</p> <p>Repairing and fabricating incidental to a retail store are permitted as an accessory use.</p> <p>Tire and battery repair and replacement as an accessory use to retail sales is permitted in the DC zone only</p>
Riding Academies, Barns and Stables		SP					
Solar Energy, Roof Mounted	P	P	P	P	P	P	See Article 7, Special Regulations
Solar Energy System, Large		SE	SE				See Article 7, Special Regulations
Solar Energy System, Small		SP	SP				See Article 7, Special Regulations
Solid waste, recycling, transfer station facilities			SE				Including storage and maintenance of vehicles and refuse containers, but excluding dumping and/or disposal on-site of waste originating off-site

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Training Facilities		SE	SE	SE			
Veterinary Hospitals and Boarding Kennels			SE				For the treatment and boarding of small animals, primarily cats and dogs, with all facilities housed inside a building with a limited outside fenced area for exercising and training with necessary office and service space
Warehouse Type Uses - All warehouse uses listed below are subject to the criteria of Article 7 Special Regulations, Section 7.24. See Sec 7.24.B to determine how separating distances are measured							
Warehouses			SP/SE*				*Warehouses > 40,000 sf, adjacent to a residential zone permitted by special exception and must be a minimum of 500 feet from a residential zone
Self-Storage facility		SP/SE*	SP/SE*				*Any facility proposed adjacent to a residential zone permitted by special exception. Individual storage spaces which open to the exterior shall be screened from roadways and residential zones. Commission may determine hours of operation.
Distribution facility			SE				All new distribution facilities must be a minimum of 500 feet from a residential zone.
Bus/truck storage facility			SE				All new bus/truck storage facilities must be a minimum of 750 feet from a residential zone
Last Mile Delivery facility			SE				All last mile delivery facilities must be a minimum of 750 feet from a residential zone
Truck and Freight Terminal			SE				With the right to service, maintain and repair motor vehicles incidental to the afore- said use All new freight terminals must be a minimum of 1,000 feet from a residential zone.
Wholesale sales and inventory directly related thereto			SE				
Wholesale sales and inventory directly related thereto for the public		SP					

4.1.2 Other Permitted Uses

Other uses or services may be approved by the Commission when the Commission determines such use or service to be substantially similar to the uses permitted above as to the type of business or as to the services offered; or as determined by the Commission to meet the purpose and general concepts of this regulation, including compatibility with the character of the neighborhood. Uses similar to those permitted by Site Plan approval may be allowed by Site Plan approval. Those uses similar to Special Exception uses may be permitted by Special Exception approval.

4.1.3 Accessory Uses

Parking and accessory uses serving the uses above are permitted. Such accessory uses of land or structures shall be intended and designed only for the use, maintenance or operation of the property.

4.1.4 Accessory Structures

Accessory structures of 500 square feet or less may be permitted through issuance of a Zoning Permit. Larger accessory structures require site plan approval. Storage of hazardous materials in an accessory structure requires site plan approval regardless of structure size. Accessory structures such as air conditioning units and trash containers must observe required yard setbacks.

4.1.5 Traffic Requirements

A traffic study or traffic impact statement may be required based on the size and type of the development. The primary requirement to perform a traffic analysis is based on the trips generated by the site, the type and size of the development and other site characteristics. When a proposed land development generates traffic that may impact traffic flow, safety, etc on the nearby street network during the peak traffic hour, an analysis is required.

The goal of the study is to demonstrate that the orderly flow of inbound and outbound site generated traffic minimizes the inherent conflicts between outbound left and inbound left maneuvers, and to must demonstrate to the Commission's satisfaction that the site generated traffic is able to enter and exit the site safely without disruption to the external traffic flow.

- A. On-site queuing provisions must be adequate to prevent site generated traffic from queuing on public streets. Sight distance for existing and proposed traffic entering or exiting the site drive must be satisfactory for the prevailing speed of approaching traffic in both directions. In the event that off-site queuing extends on to the public way, the owner shall be responsible to provide the Commission with a remediation plan to bring the site back into compliance. This may require a modification to the site to accommodate the additional traffic volumes. The owner shall be responsible for costs associated with traffic control measures needed.
- B. Design provides for safe and orderly vehicular and pedestrian flow and movement of traffic and minimizes vehicular and pedestrian conflicts. Delivery areas must be located so that they do not conflict with normal operations.
- C. When required, a traffic report must be provided which addresses the following: traffic impact analysis including present roadway conditions including geometry and radius to handle anticipated truck traffic, existing and projected traffic volumes (average daily traffic, A.M., P.M. peak hours) existing volume capacity ratios, existing and projected levels of service at the site driveway and all nearby intersections, site generated traffic and distribution, and accident experience. Impact on streets that may be affected by the proposed development shall be identified.
- D. Drive thru facilities shall be designed to accommodate adequate queuing, including a bypass lane, for a minimum of 15 vehicles unless otherwise determined by a traffic analysis, specific to the proposed site use.
- E. To provide safe flow for the public and eliminate conflicts on site, and inbound and outbound traffic to and from the site, the applicant must adhere to the ASHTO turning radii for both WB62 and WB67 Class vehicles when such vehicles will be accessing the site.

Section 4.4 I-291 Corridor Development Zone (CD)

4.4.3 Permitted and Prohibited Uses

Permitted uses are those commercial, office, hotel/motel and industrial uses that reflect the purposes of these regulations by careful attention to the appearance and siting of their buildings and/or site amenities, and that meet all of the performance standards enumerated within this section and all other applicable sections of these regulations, with the exception of the following specifically prohibited uses:

1. Gasoline service stations/automotive sales, service and repair garages;
2. Casinos, arcades (amusement centers with coin-operated games);
3. Adult-oriented businesses (including but not limited to adult bookstores, adult theaters, massage parlors, etc.);
4. Nightclubs, taverns, social clubs, and similar uses where the primary purpose of the business is the serving of alcoholic beverages;
5. Any use similar in purpose, use, or service to the above-prohibited uses, regardless of its' name or title; and
6. Residences/residential uses.

Any warehouse type uses defined in Section 7.24 Freight, Truck, and Bus Terminals; Warehouse and Distribution Centers are not subject to the separating distances from residential zones.

4.4.6 Parking and Circulation Requirements

Off-street parking shall conform to the requirements of Section 6.4 Parking and Access – Off-Street Parking and Loading, as well as to the following.

A. General Circulation Requirements

Site plans shall conform to the guidelines and recommendations of the Town's access management program. Circulation and access layouts will be evaluated for functional efficiency and safety. Considerations will include limited driveway cuts along Route 30 (Ellington Road) and the incorporation of shared access between adjacent lots and consolidated access for contiguous lots.

B. Pavement Size

Contiguous pavement areas for parking (exclusive of islands) shall not exceed 1 acre, at which point a major island division will be required. "Major island division" is a planted median extending the longest dimension of the one-acre paved area and a minimum of 20 feet in width. Mounding is encouraged, provided it does not interfere with sight lines.

C. Parking in Yards

1. There will be no parking permitted in required **zoning setbacks** abutting Route 30 or I-291.
2. Side and rear yard parking are encouraged for all business and industrial uses. **Employee parking, to the maximum extent possible, shall be located adjacent to buffers which abut**

residential zones.

3. Front yard parking, as the dominant parking area, will be considered for commercial and service-oriented developments only.

Section 4.5 Industrial Zone (I)

4.5.1 Purpose and Intent

The primary purpose of this zone is to provide areas which permit manufacturing and related uses, including large- and small-scale warehousing, office, service industries and distribution facilities, which by design, are compatible with abutting zones and uses. The district is intended to provide industrial facilities interspersed with other uses and with the utilities and infrastructure necessary to support such industrial operations. The provisions of this zone are designed to recognize the need for manufacturing space while ensuring that these areas are compatible with adjacent residential neighborhoods and with the capacity of available infrastructure. Access management, strong design and sign control, adequate buffering treatments, and compatibility with abutting residential uses must be included in all site plan and/or special exception applications for Commission approval.

4.5.2 Establishment of Zone

The Industrial Zone shall be established by the Commission only after taking into account the various factors favorable and unfavorable to such a change, including but not limited to those criteria listed in Section 8.3 Zone Change Standards and Procedures.

4.5.3 Accessory Uses

Accessory uses normally incidental to the principal use, including garages, storage buildings, and power plants, as well as employee recreational, commissary and clinical facilities are permitted uses. Incidental sales shall be permitted as an accessory use provided that: (a) no more than 20% of the gross building floor area is devoted to such sales, or (b) in a landscape materials sales and storage use, not more than 20% of the gross exterior display/storage yard is devoted to such sales, or (c) not more than 6 sales events are held per year. Cannabis production facilities, including micro-cultivators, are prohibited from creating showrooms and/or retail sales on the site.

4.5.4 Outdoor Storage of Material

Outside storage shall be limited to materials and equipment used in on-site manufacturing, processing or assembly and to goods produced on site and awaiting shipment, provided that: outside storage shall be limited to a maximum height of twenty (20) feet, shall be confined to the rear and side yards only in relationship to the building front; and shall be effectively screened from view from adjacent properties and public ways.

See Section 2.11 Commercial and Industrial Storage and Display for outdoor storage requirements and Section 6.2.5 for Screening requirements

4.5.5 Rail

Rail spurs and sidings shall be located no closer than 150 feet from a residential zone, and are prohibited within buffer areas. The above may be **modified** by the Commission in cases where the proposed rail spur or siding will not, in the opinion of the Commission, cause undue noise or visual disruption, or safety hazard, to abutting residential zones or uses. The Commission may require special screening and/or noise abatement measures for rail sidings.

4.5.7 Pedestrian and Bicycle Access

All site plans shall accommodate bicycle and pedestrian access as outlined in Section 6.7 Pedestrian and Bicycle Accommodations.

4.5.8 Performance Standards

a. Environmental Impact.

No use shall be allowed that is noxious or offensive by reason of the emission of smoke, particulate matter, noise, dust, glare, fumes, odor, ionizing radiation, vibration, heat or any other pollutant or waste. All industrial uses which may potentially emit such pollutants, shall submit a written assessment of the environmental impacts of the proposed uses and a plan which demonstrates how the project will comply with local, state and federal environmental regulations.

b. Noise.

Noise shall be controlled by design (sound wall placed close to the source of the noise) and/or vegetative screening to minimize impacts on adjacent streets and properties. In accordance with the provision of Section 22a-174-18 of the Connecticut State Statutes, and Town Ordinance Article II Noise Control, no truck engine shall be allowed to idle for a period in excess of three minutes, when such vehicle is parked in any parking lot, truck loading area, transient loading space, or other location adjoining a residential area. Property line noise ordinance limits apply to vehicles and equipment moving within the property lines.

c. Air Pollution.

In order to minimize the pollution of air, all uses shall comply with the standards and requirements of the vision of Section 22a-174-1 to 22a 174-200 of the Connecticut State Statutes inclusive and all other applicable federal, state and local laws.

4.5.9 Illumination

Exterior illumination and noise shall be controlled by design or screening so as not to intrude upon adjacent streets and properties and subject to Section 6.1 Lighting Standards. Reduction of light during overnight hours and/or after the close of business is encouraged and may be required as a part of a special exception approval.

4.5.10 Protection of Natural Diversity

No land located within an area designated by the Connecticut Department of Energy and Environmental Protection (DEEP) as “Listed Species and Significant Natural Communities” as amended, may be approved for site development or subdivision without first applying to the DEEP for an NDDB property review.

Section 6.2 Landscaping and Buffers

6.2.1 General Landscape Standards

- A. Suitable landscaping, including lawns and nursery-grown trees and shrubs, designed to be low maintenance wherever possible, is required in all areas not covered by impervious surfaces. The Commission may allow existing natural vegetation to be retained as part of the approved landscaped plan.
- B. **Invasive plant species identified by the State of CT Department of Energy and Environmental Protection (DEEP) shall not be planted nor included on any landscaping plan.** This list can be found at: <https://portal.ct.gov/DEEP/Invasive-Species/Invasive-Species> **(or subsequent page)**
- C. Site landscaping shall be designed in accordance with the guidelines in Section 8.7 Architectural and Design Review, and shall be designed to maximize site compatibility with the surrounding neighborhood.
- D. Stormwater detention basins that are visible from a street (including private streets serving the public) must be attractively designed and landscaped, with side slopes no greater than 4’V:1’H for maintenance purposes. **Fencing may be required for safety purposes. Decorative fencing around the basin is required if the fencing is visible from the public way.**
- E. Large trees and stands of mature trees and shrubs are to remain undisturbed where practical and desirable. Existing healthy trees of 18-inch caliper and greater (to be survey-located either singly or as groups) shall be incorporated into the site plan to the maximum extent possible. Similarly, significant stands (10 or more trees) of related species, or consistent scrub-shrub groupings occurring in front, side or rear yards shall be preserved whenever feasible. When possible, existing trees shall be saved by appropriate welling and mounding.
- F. All trees and shrubs to remain undisturbed shall be tagged, or otherwise identified, in the field prior to commencement of site work, and shall be shown on the landscaping plan.
- G. **Landscaping plans shall incorporate crop pollinator habitats into their designs. See Appendix I, Sec 11.1.9.D.**
- H. **Landscape strips that are left in a natural state as well as landscaping areas- shall be regularly monitored and cleared of trash.**

6.2.2 Maintenance of Landscaping

All landscaping elements portrayed on the approved landscaping plan, including buffer treatments, shall be maintained in a manner sufficient to ensure **their** continuing performance and the survival of plantings. All landscape bonds shall be held two years and inspected prior to release of the bond. **On sites developed without irrigation systems, the Commission may hold the bond for an additional year to ensure survival of plantings.**

All buffer bonds shall be held a minimum of two years. At that time, the applicant may request a reduction to 25% of the original amount to be held for an additional 2-3 years to ensure survival of the buffer plantings. A letter shall be submitted by the landscape architect verifying the plantings with the reduction request.

Where a maintenance problem arises, upon order of the Zoning Enforcement Officer, said landscaping shall be restored to a satisfactory condition consistent with the approved landscaping plan.

6.2.3 Obstruction by Landscaping

Landscaping shall not obstruct line-of-sight for vehicles entering and exiting the premises, nor shall it unduly obstruct line-of-sight for vehicles traveling on abutting Town or State highways. Corner visibility shall be maintained consistent with Section 2.8 Corner Visibility. Landscaping shall not obstruct line of sight for vehicles entering and leaving either common driveways or other access ways. **Sight lines shall be shown on the plans in accordance with AASHTO speed guidelines.**

6.2.4 Buffers

A. Purpose

The purpose of buffers is to mitigate visual, noise, **pollutants** and other impacts between residential and non- residential uses, and between low-density and high-density residential uses. **Buffers shall be designed to: enhance visual interest; screen undesirable views; screen undesirable noise; filter air pollutants and odors; and separate human activities. See Appendix I for more information on buffer descriptions and designs.**

B. Standards and Design

1. For the purposes of this section, a site may mean an entire lot; or, when a parcel is to be developed in phases, a portion of the lot that bears a reasonable relation to the building(s) for which a site plan has been approved.
2. Wherever any non-residential use/zone abuts any residential zone, a buffer shall be required. The buffer shall meet the following criteria:
 - a. The field of view between abutting residential and non-residential uses/zones shall be obscured visually within 5 years time to such an extent that activity on the abutting lot is not immediately apparent. The field of view between abutting residential and non-residential uses/zones shall be substantially obscured visually by the maturity of the planting. **At the time of the application, this shall be illustrated through cross-sections showing the buffer at the time of planting, at five years and again at maturity.**
 - b. Plantings shall be staggered/clustered to achieve maximum screening after 5 years, and at maturity. Evergreen trees shall be a minimum of **8-10** feet in height at the time of plantings; deciduous shade trees shall be a minimum of **3"** caliper and **10-12** feet in height at the time of planting; and flowering trees shall be a minimum of **8** feet in height at the time of planting and **2 1/2"** caliper. **See Appendix I, Section 11.1.9.2 Cross section illustrations.**
3. Wherever any non-residential zone abuts any residential zone, a buffer is required on both sides of the shared zone boundary line. Applicants are responsible for providing the required buffer on their side of the zone boundary. Buffers are not required along street frontages; however, the Commission may require screening. Buffers are not required between 2 abutting

non-residential uses in residential zones. **See Sec. 6.25 Screening Requirements**

4. The following special exception residential uses require a buffer along all property boundaries except street frontage:
 - a. Multifamily (MF-A, MF-AA)
 - b. Elderly Housing (Section 7.7)
 - c. **Multi-family Assisted Housing (Section 7.22)**
 - d. **Senior Residence Development (Section 7.17)**

When two of these special exception uses abut, the field of view between the two uses shall be substantially reduced but not necessarily obscured, both within 5 years' time and at maturity.

5. All buffers are in addition to required yards (building setback lines).
6. All buffer areas shall be designed by a licensed landscape architect.
7. Clear cutting/harvesting of trees within a buffer area is expressly prohibited at any time without prior Commission approval.
8. The Commission may modify proposed landscaping plans to require more mature plantings, different species, or alternative design, in order to afford a functional and aesthetically pleasing buffer area. **In areas where there are significant grade changes between uses being buffered, the buffer design may propose an alternative buffer to include plantings closer to the activities as well as along the property line. The landscaping treatments shall meet the intent of the design and performance standards of 6.2.4.B.**
9. Trees shall not be planted within 20 feet of a sewer line or area of heavy equipment use. **Low growing vegetation may be permitted (e.g. bushes no higher than 3 feet in height).**

C. Buffer Widths

Table 6.2.4A Buffer Widths

Zone/ Use	Buffer Width
GC, RC, TS	50 feet
I	75 feet; Applicant may request reduction to 50 feet in accordance with Sec. 6.2.4.C.1 and 2 below
GD, CD	75 feet
DC	100 feet
RO	25 feet

MF; Elderly Housing (Section	25 feet
RR, AA, A	50 feet
RROZ	25 feet
Non-Residential Uses in Residential Zones	50 feet
Assisted Living	50 feet
Independent Living	50 feet

1. In an industrial zone, pre-existing lots developed or approved on the land records prior to 5-28-23 can maintain a 50-foot approved buffer (as previously required).
2. When a 75-foot buffer is required, the buffer width can be reduced to 50 feet upon demonstration that the buffer design meets performance standards identified in Appendix I, Section 11.9.1.A, B and/or C. Approval by 2/3 majority of commissioner's is required for a buffer reduction.
 - a. The **dimensions** of the buffer to be planted prior to issuance of the first Certificate of Occupancy (CO) will be so indicated on the site plan of development for industrial/commercial and high- density residential developments and on the individual plot plan for single-family residential development.
 - b. The Commission strongly recommends that developers begin installing the entire buffer on a parcel well in advance of planned building activities for the balance of the parcel, as smaller plantings can be utilized. The buffer may then exceed the required specifications at the time a CO is requested.

D. Buffer Plantings

Areas to remain undisturbed during construction activities (i.e., non-encroachment areas) shall be indicated on the landscaping plan. If filling, cutting, grading, or other disruptive activities occur near non- encroachment areas, trees and shrubs to be preserved shall be protected in accordance with accepted landscaping practices. The methods proposed to afford such protection shall be identified on the landscaping plan or in a corresponding document. The buffer shall be designed to meet or exceed the performance level described in Sec. 6.2.4 herein.

When mature existing vegetation is not being incorporated into the buffer plantings, or when such vegetation comprises a relatively insignificant proportion of the buffer, the buffer area shall be composed of a suitable combination of evergreen, deciduous, flowering trees and shrubs to meet the criteria listed in paragraph 6.2.4.A

E. Alternative Buffer

The Commission may **allow a reduced buffer width** with an alternative landscaped buffer design that meets, or exceeds, the performance level of the **buffer plantings**. Said alternative buffer shall include tree and shrub plantings, and may include hedges, earthen berms, fencing, **walls** or other treatments. **Man-made barriers, such as walls, fences, etc, shall be designed to not impede wildlife movement through the**

barrier. See Appendix I.

F. Berms

Berms may be used as an integral part of a buffer and/or screening plan. The following standards shall apply to all berms. See Appendix I for illustrations

1. No structures, including fences and walls, shall be placed on a berm unless as part of the landscaping requirements for a development site;
2. If included in the landscape design, berms shall:
 - a. Have a minimum height of 2 feet, a minimum crown width of 2- 4 feet, and a side slope with a typical width to height ratio of no greater than 3 to 1 (unless site conditions warrant a steeper berm).
 - b. Be designed and constructed with an undulating appearance which mimics, as much as is practicable, a natural topographical feature of the site.
 - c. Be substantially planted and covered with live vegetation. No berm shall consist entirely of turf grass, ground cover, mulch or similar material.
 - d. Be fully installed, planted, stabilized and maintained prior to issuance of a certificate of zoning compliance or occupancy.
 - e. Be designed to prevent standing water and not to impede the flow of stormwater on the property or from adjacent properties.
 - f. Be designed to not impede wildlife movement through the barrier.

6.2.5 Screening Requirements

A. Purpose

An opaque screen is intended to provide a visual screen and reduce noise from the feature, land use or activity from any protected property, public street, or right-of-way. An opaque screen may be composed of a wall, fence, building, berm, or a combination thereof, subject to Commission approval. Natural areas, as detailed below, may also be used to screen land uses or activities.

B. Standards for Screening

1. Natural areas for screening - Where an existing vegetated area is located on the same property as the proposed development; is within or includes the required buffer; and is of sufficient height, length and depth and contains adequate and sufficient healthy vegetation to provide a visually opaque screen year-round as required in this section, no further improvements shall be required. Such area must remain intact and be protected throughout all phases of development, including any land disturbance.
2. Height of required screening - The height of required screening shall be sufficient to block the view of the feature, land use or activity for which the screening is required

with accommodations for reasonable access, use, and maintenance of the features and equipment, as necessary.

3 Mechanical equipment for all uses other than single-family residential and duplexes at ground level and mounted on roofs, including, but not limited to HVAC equipment, transformers and generators shall be screened. The length of a required screen shall be that which is necessary to screen the feature, land use or activity from protected properties, streets, and rights-of-way as provided in this section, however screening cannot obstruct the line of sight for vehicular traffic and must comply with the requirements of Corner Visibility, sight triangle requirements.

4. Roof mounted mechanical equipment shall not be visible in any direction from roads and public rights-of-way. Screening of roof-mounted equipment shall be accomplished by solid and permanent roof-mounted screens, compatible with the architectural style, materials and color of the building upon which the equipment is located. Where it can be clearly demonstrated in the application that such equipment is not visible from any adjacent existing residential properties, properties in residential zoning districts, roads and public rights-of-way, the rooftop screening will not be required. This will be verified at the time of the Certificate of Occupancy and screening measures may be required if it is determined that objects are visible.

C. Types of Screening

1. Fences, walls and/or chain-link fences with strips composed of wood, plastic, metal may be used in meeting screening requirements.

2. Anti-graffiti coating or an equivalent measure to prevent graffiti shall be provided for all screen walls visible to the public.

Section 6 Off Street Parking

Table 6.4.3B Minimum Required Parking Spaces – Commercial and Industrial Uses

Use	Minimum Required Parking Spaces
Bar, Nightclub, Lounge	1 space per 50 sq. ft. of Gross Floor Area (GFA)
Bowling Alley	4 spaces per lane plus any restaurant requirements
Business Offices <50,000 sq. ft. of GFA	4 ½ per 1000 sq. ft. of GFA
Business Offices 50,000 - 100,000 sq. ft. GFA	4 per 1000 sq. ft. of GFA
Business Offices >100,000 sq. ft. of GFA	3 ½ per 1000 sq. ft. of GFA
Car Wash	10 spaces per stall including stacking
Church, Synagogue	1 space per 3 seats
Distribution Facilities (including wholesale and/or warehousing)	1 space per 1,000 sf up to 50,000 sf; 1 space for each 2,000 sf over 50,000 (other uses to be calculated in addition)
Fast Food	1 space per 50 sq. ft. of GFA
Financial Institution	1 space per 250 sq. ft. of GFA plus 1 per employee
Freight Terminals	
Hotel, Motel, Tourist Home	1 space per unit plus restaurant requirements
Hospital, Sanitarium, Convalescent or Nursing Home	1 space per 3 beds, plus 1 per 2 employees
Manufacturing	1 space per 700 sf plus 1 space per employee based on largest shift plus 20% for shift change
Library	1 space per 400 sq. ft. of GFA
Medical and Dental Office	4 spaces per 1000 sq. ft. of GFA
Places of Assembly, Amusement, Recreation, and Education	1 space per 250 sq. ft. of GFA
Research	1 space per 700 sq. ft. GFA or 1 space per 2/3 employees, whichever is greater
Restaurants, Taverns, Cocktail Lounges	1 space per 50 sq. ft. of GFA
Retail Stores, Personal Service Shops	1 space per 200 sq. ft. of GFA
Roadside Stands	3 minimum, plus dwelling lot requirements
Shopping Center <200,000 sq. ft. of GFA	4 per 1000 sq. ft. GFA
Shopping Center 200,000 - 500,000 sq. ft. GFA	4 ½ per 1000 sq. ft. of GFA
Shopping Center >500,000 sq. ft. GFA	5 per 1000 sq. ft. of GFA
Theater	1 space per 3 seats
Undertaking Establishments	1 space per 50 sq. ft. of GFA
Warehouse (including self-storage)	1 space per 1,250 sq. ft. plus 1 space per employee based on largest shift plus 20% for shift change
Each use not listed above	The Commission shall determine the appropriate number of spaces based upon similarity to the above uses and/or standards available from recognized authorities.

6.4.4 Off-Street Parking – General Provisions

A. Shared Parking

In the case of more than one use of a building or lot, required parking facilities shall be construed to be the sum of the requirements for all uses computed separately according to the above schedule.

The parking requirements are exclusive of any truck parking requirements on site.

B. Evening Use

Parking facilities for a predominantly evening use may be considered as part of the facilities for daytime use where:

1. Written agreement to such mutual use is recorded in the Office of the Town Clerk.
2. The Commission, after public hearing, approves such mutual use and establishes such safeguards as will carry out the purpose of this section.

C. Reserve Parking

The Commission will consider "reserve parking" arrangements where the number of spaces proposed is less than that required by these regulations, provided that the applicant can demonstrate to the Commission that the required spaces are excessive and are unnecessary. Reserve spaces shall be shown on the site plan and such area shall count towards impervious coverage requirements.

D. Changes in Minimum Parking Requirements

Whenever, after the date of these regulations, there is a change in the number of employees or business visitors, the lawful use of the premises, or any other unit of measurement specified in any of the foregoing paragraphs of this section, and whenever such change creates a need for an increase or decrease of more than 15% of the number of off-street parking spaces as determined by the requirements of this section, more or less off-street parking facilities shall be provided within a reasonable time on the basis of the adjusted needs, as determined by this section.

E. Building Expansions

Parking facilities off the street or highway right-of-way shall be provided to serve all buildings erected, moved, altered, or enlarged in excess of 25% of the present floor area of dwelling units, and all premises otherwise developed after the adoption of these regulations. Such facilities shall be sufficient to accommodate the motor and other vehicles of all occupants, employees, customers, and other persons normally visiting any building or premises at any one time. The pavement, drainage, landscaping, and lighting of parking facilities shall be maintained in proper condition so that the facility may function as designed.

F. Shared Access

Two or more parking facilities on adjoining lots, if designed for use as a single parking area, may use the same means of access.

G. Handicapped Parking

Handicapped parking shall be provided in accordance with the requirements of the Connecticut General Statutes and shall be located in a convenient location relative to the entrance to a building. **ADA requirements for EV charging stations must be accommodated**

H. Emergency Access Ways

All buildings shall be provided with access ways suitable for emergency vehicles subject to review and approval from the Fire Marshal. Depending upon the use and location of a building, it may be required, subject to review and approval of the Fire Marshal, that such access ways completely surround a building.

I. Circulation Road

Any sites with parking lot(s) in excess of 200 total spaces shall be designed with a circulation drive around the building(s) that has a minimum lane width of 20 feet (for one-way pattern) clear of parked vehicles and other obstructions.

J. Construction of Parking

1. All parking spaces, parking lots, and driveways shall be paved with bituminous concrete, concrete, or other suitable surface material **and provide for proper drainage.**
2. Final pavement for parking and drives that are to serve a particular building shall be in place prior to issuance of the Certificate of Occupancy for that building.
3. All crosswalks, sidewalk access, and parking accommodations shall be in conformance with **current ADA Requirements (ADAAG) and CT GS 14-253a.**

6.4.5 Design of Parking Areas

- A. The width, location, and arrangement of driveways and other access ways and parking shall be consistent with public safety and welfare, and shall provide no undue hindrance to fire or other emergency apparatus, or to the safety of existing or proposed ways. **A turnaround area may be required on site to ensure safe turnaround for the anticipated size of trucks utilizing the site. This turn around should be shown on the proposed site plan.**
- B. Off-street passenger car parking and circulation is permitted within required yards. Circulation (except for emergency and public safety vehicle access) and parking of trucks, forklifts, and other commercial vehicles and equipment is prohibited within required yards abutting a required buffer area.

Table 6.4.5A Parking Area Design

Type of Aisle/Driveway	Minimum Width of Driveways
Two-way traffic streets or access ways, including those between parking spaces and in parking lots	24 ft
One-way traffic access ways (driveways), with parking on 1 or more sides, and contiguous to said access way, with:	
45-degree parking on 1 side	16 ft
60-degree parking on 1 side	18 ft
90-degree parking on 1 side	24 ft
Parking (at any angle) on both sides	24 ft

- C. Individual parking spaces that are accessed from the main **access** driveway are not allowed.
- D. Any parking area designed or intended for use by 3 or more vehicles, which is located and adjacent to any public sidewalks or the planned location of a public sidewalk, shall be separated from such sidewalk by a suitable barrier so placed as to prevent the encroachment or parking of automobiles on such public sidewalk or planned location thereof.
- E. All off-street automobile parking facilities shall be designed with appropriate means of vehicular access to a street or alley as well as maneuvering areas. The Commission shall determine the appropriate curb cut during the site plan review process. The Commission shall consider the provisions of Section 5.5.4 Access Provisions, in determining the suitability of a proposed curb cut location.
- F. Parking spaces shall conform to the following:
 - 1 Dimensions shall be 9 feet by 18 feet, striped with double striping, with at least 18 inches between stripes. Single striping may be permitted for small, low-turnover parking areas.
 - 2 Each space shall be of useable shape and exclusive of driveways and access areas.
 - 3 No area shall be credited as a parking space that is in any part also credited or used as a loading space.
- G. Handicapped parking spaces shall generally be associated with handicapped-accessible units or with building entrances designed for handicapped access.
- H. In high-density residential development, driveways servicing individual garages shall be at least 20 feet in length and 11 feet in width. Where no sidewalks exist the 20-foot length shall be measured from the face of the garage door to the curb line. If there are sidewalks, the 20-foot length shall be measured from the face of the garage door to the nearest edge of the sidewalk.
 - 1 Dimensions shall be 9 feet by 18 feet, striped with double striping, with at least 18 inches between stripes. Single striping may be permitted for small, low-turnover parking areas
 - 2 Each space shall be of useable shape and exclusive of driveways and access areas.
 - 3 No area shall be credited as a parking space that is in any part also credited or used as a loading space.
- I. **Wider truck lanes shall be designed to reduce conflicts where trucks would cross into opposing lanes.**
- J. **Turning radius from the public/private road into the parking area shall be designed in accordance with the Town of South Windsor Public Improvements Specifications. The plans should demonstrate that two vehicles, of the largest size expected to use this site, can pass in opposite directions safely based on turning templates (unless one-way traffic pattern is proposed)**
- K. **Sizes of parking spaces for different vehicle types shall be identified on the site plan and signage provided to easily identify onsite parking spaces (e.g. cab parking, trailer parking).**
- L. **No parking shall be designed that will interfere or block any other designated loading areas. Employee parking shall be accessible without intermingling with truck traffic coming into and from the site. A separate employee entrance may be required to avoid conflicts.**
- M. **All parking lots shall accommodate Electric Vehicle Charging stations in conformance with Section 6.4.10.**

6.4.6 Parking Lot Landscaping

- A. Parking lots shall be landscaped with shade trees which shall be continuously maintained. The parking lot requirements of this section apply to all developments and zones except where specific parking lot landscaping requirements are included elsewhere in these regulations.
- B. Perimeter landscaping is required around all parking areas. Planting along the perimeter of a parking area (including screening, landscaping, or buffering) will not be considered as part of the interior landscaping requirement. Terminal peninsula planting areas at the ends of rows of parking within any perimeter access drive may be counted toward the interior landscaping requirement as long as each such peninsula has a minimum dimension of at least 8 feet in its narrowest dimension and contains at least 1 tree.
- C. For landscaping purposes, ground cover alone is not acceptable. Trees should be selected for shade and adaptability to parking lot conditions.
- D. Deciduous shade trees shall be a minimum of 2" caliper and 10 feet at planting; flowering trees shall be a minimum of 6 feet in height at the time of planting and 1½" caliper.

Table 6.4.6A - Minimum Amount of Required Landscaping

Parking Area Element	Amount of Landscaping	Additional Requirements
Islands (mid-bay and terminal bay locations)	None specified	Minimum dimension of 8 feet in any direction
Terminal peninsula planting areas	At least 1 tree	Minimum dimension of 8 feet in any direction
Street frontage and perimeter screening	1 tree or 2 shrubs or a 5' x 5' planting bed (or any equivalent combination) for every 3 perimeter parking spaces	Minimum width of 10 feet The Commission may waive up to ½ of this requirement for excellence in the proposed planting plan in terms of the variety and size of the proposed plantings
Parking area with fewer than 30 parking spaces	5% of interior parking area	1 tree for each 10 spaces
Parking area with more than 30 parking spaces	10% of interior parking area	1 tree for each 10 spaces

E. Location of Landscaping

The landscaping should be located in protected areas, such as along walkways, in center islands, at the end of bays, or between parking stalls. **Incorporation of rain gardens and/or biofilter swales within the parking lot islands is encouraged.**

F. Other Provisions

1. Planting beds must be distributed as evenly as possible throughout the parking area.
2. The parking and landscaping plan shall show a satisfactory method of storm drainage and snow storage, and planting beds shall generally be protected by curbing.
3. In order not to alter excessively the groundwater table, the applicant is encouraged to integrate water detention areas, and to include rain gardens, within the overall design of the parking lot.

G. Preferred Plantings

1. Trees in parking areas shall be deciduous, large, shade variety, with small leaf characteristics. Suggested species include: Ash, Honey Locust, Zelkova, Callery Pear, Red Maple, Red Oak, and Littleleaf Linden.
 1. For landscaping purposes, ground cover alone is not acceptable.
 2. Shrubs and other plants shall be selected for seasonal interest and color, and tolerance to parking lot conditions. Trees should be selected for shade and adaptability to parking lot conditions. Trees and bushes planted within 5 feet of any parking area shall be of a variety capable of withstanding salt damage.
 3. Preparation of beds for trees shall be described. Mulched planting beds shall be provided around all trees and shrubs.
 4. Gravel or stone shall not be used for ground cover unless suitably contained within the intended area.
 5. Parcels bordering conservation easements and/or regulated wetland areas are encouraged to include shrub and tree plantings that are wildlife attractive. Suggested plantings include the following species: Downy Shadblow, Witchhazel, Chokeberry, Viburnum (in variety), Barberry, Inkberry, Winterberry, and Honeysuckle.

6.4.8 Off-Street Loading

6.4.8.1 General Provisions

A. Loading and Screening

1. The number of loading spaces shall be sufficient to accommodate the anticipated loading requirements of building users, which requirements shall be in accordance with industry practice, but not to exceed said requirement. **Industrial and warehouse truck loading aprons shall be no closer than 150 feet from any residential zone boundary line.**
2. **Existing industrial and warehouse use- facilities that do not comply with the requirements of this section may be expanded provided that changes to the truck loading apron do not increase the non-conformity.**
3. **Truck loading spaces and truck loading area aprons shall be designed wide enough and located so that their use does not impede the normal use of required parking spaces or accessway. Trucks shall be able to access the bay without multiple maneuvers.**
4. **Except for short-term truck loading spaces, to the extent practical, truck loading areas shall be surrounded by complete visual screening and/or sound reduction wall so as not to be visible from front yard or adjacent properties or from the public street.**

B. Loading Docks/Receiving Areas

1. Loading docks and receiving areas shall be designed as an integral part of the building and/or adequately screened such that these areas do not adversely impact the site and building aesthetics.
2. **Truck loading areas located between a building and the adjoining street are prohibited except**

for properties containing more than one frontage, the Commission may allow truck loading area aprons between the sides of the building and a secondary frontage, provided that no apron is located within a required front yard, or opposite a residential zone and that the apron is screened in accordance with Section 6.2.5.

3. Existing industrial and warehouse use facilities that do not comply with the requirements of this section may be expanded provided that changes to the truck loading apron do not increase the non-conformity.
4. Sufficient on-site vehicular maneuvering area must be provided; trucks destined to, or coming from, loading docks shall not require maneuvering movements within the public highway. Access/egress for loading areas shall be designed to provide adequate, safe maneuvers in a manner that does not compromise the efficiency and safety of parking areas. Truck maneuvers on streets/drives, other than entering and exiting, are not permitted.
5. Where lots abut a residential or rural zone or use, loading docks shall not face said zone or use.

C. Protection of Buildings

To limit damage to buildings and structures in the vicinity of short-term truck loading spaces, truck loading area aprons, fuel pumps, and drive through windows/lanes:

1. Bumpers shall be required at dock-height doors and bollards shall be located to either side of the drive-through doors, at building corners adjacent to paved accessways, or adjacent to drive through windows to absorb potential damage from vehicles;
2. Walls adjoining truck loading area aprons, drive -through window/lanes, or other paved areas not separated from the building by a required landscape buffer shall be constructed of brick, masonry block that is sound attenuating, resistant to denting, easily repaired and compatible with other materials and design of the overall building
3. The bottom of truck loading dock and fuel pump canopies shall be a minimum height of 14 feet and the bottom of drive-through canopies shall be a minimum of 10 feet in height.

D. Loading Space Standards

1. Any new building or addition, 10,000 sf or more, that is to be used for manufacturing, storage, retail, wholesale sales or other similar use where materials are received or distributed by truck, the following minimum required off-street loading spaces shall be provided and permanently maintained on the site for each prospective use:
 - a. One (1) space for each use between 10,000 – 20,000 sf and one (1) space for each additional 10,000 square feet or part thereof;
 - b. Each truck loading space shall be at least 10 feet in width and 30-40 feet in length (based on expected trucks);
 - c. Each truck trailer storage space shall be at least 12 feet in width and 55 feet in length and shall not be considered a required truck loading space;
 - d. Sites shall be designed to minimize conflicts between delivery vehicles and parking spaces.

6.4.9 Modification of Minimum Required Parking Spaces

- A. A reduction in parking spaces may be allowed when the Planning and Zoning Commission deems the reduction to be in the best interest of the Town, according to the following:
1. The increase in stormwater run-off rate shall be held to a minimum by reducing the parking spaces, and/or
 2. The applicant demonstrates through actual experience that a lesser number of parking spaces will suffice, and further that due to the nature of the building or business, future owners/occupants of the building are also unlikely to need the number of parking spaces required by the zoning regulations.
- B. The maximum reduction allowed is 10% of parking required with the option to show spaces as “reserve” spaces on the site plan and the construction of the parking area and installation of the spaces may be phased according to short to long term needs of the proposed use(s). This approach is subject to the following conditions;
1. Up to 50 % of the total required spaces may be deferred in reserve parking except that for buildings housing warehouse uses, this deferred percentage may be increased to 70%. This approval becomes invalid if the use changes.
 2. Spaces not intended for construction shall be labelled “Reserve parking” on the plan and shall be properly designed and shown as an integral part of the overall parking layout design and must be located on land suitable for parking.

Section 6.7 Pedestrian and Bicycle Accommodations

6.7.1 Purpose

The purpose of these regulations is to promote and support access by bicycle and walking throughout the town of South Windsor. All parking lots must be designed to provide safe and convenient pedestrian and bicycle access as part of any parking lot and site design, including safe and convenient pedestrian and bicycle movement to and from public walkways and/or bikeways or streets, and between developed lots and/or undeveloped lots.

6.7.2 Pedestrian Design Standards

- A. Access to individual buildings within the site shall be from a system of convenient and safe pedestrian ways and shall be incorporated into the plans for any site development plan or parking area in accordance with the following standards:
1. Pedestrian ways and sidewalks shall provide safe separation or delineation from motor vehicle traffic through the use of raised sidewalks and/or landscaping between sidewalks and parking spaces and /or driving aisles.
 2. Pedestrian crossings shall be demarcated through the use of raised pavers, textured concrete, pavement markings, pedestrian warning signs, and/or lighting as appropriate.
 3. Pedestrian ways and sidewalks may be constructed of concrete or other decorative-type paving material and shall include pedestrian warning signs and lighting, and shall not be less than four (4) feet in width. Multi-use paths at least ten (10) feet in width may be required along collector and arterial roads.

4. If concrete wheel stops are not provided at parking spaces perpendicular to adjacent pedestrian ways, pedestrian ways and/or sidewalks shall be increased by two (2) feet on each side where parking spaces are located.
5. Pedestrian ways and sidewalks may be flush with adjacent pavement where necessary to facilitate wheelchair and shopping car access if concrete wheel stops or traffic control devices including line striping and signage are provided.
6. Plantings, benches, and lighting may be provided along walkways and at pedestrian crossings.
Egress paths shall be lit for the safety of employees and visitors.
7. **Pedestrian ways and sidewalks shall be provided wherever possible for connections to adjacent lots (developed or undeveloped) or neighborhoods.** Maintenance of public sidewalks, including the clearing of snow, shall be the responsibility of the property owner. Pedestrian ways internal to the site should connect to the public sidewalk at the street.

6.7.4 Mass Transit Provision

In order to conserve energy and lessen congestion in the streets, improvements may be required in developments requiring 100 parking spaces or more to provide for bus pullouts and/or shelters. These shelters can be provided on site or along the public right of way.

Section 7.24 Freight, Truck, and Bus Terminals; Warehouse and Distribution Centers

7.24.1 Intent

The intent is to allow for wholesale, storage and/or distribution activities, truck freight and bus terminals that are compatible with neighboring uses; have adequate community services, street, utilities and will not have a harmful effect on surrounding neighborhood(s).

- A. **Warehouse use:** Where used throughout these regulations, the term warehouse use applies to warehouses, distribution centers, box truck storage facilities, freight terminals and last mile delivery facilities. See Section 11.9, Appendix J for warehouse type definitions.
- B. The separating distances established below between residential zones and proposed use(s) shall be measured by establishing a radius of the required separating distance from the residential zone to establish the boundary. All proposed activities for the use, including parking areas, truck courts, buildings, etc. shall be shown on the site plan and shall fall outside of the separating distance radius.
- C. Any existing site with an approved warehouse type use at the time of the adoption of these regulations, 05-28-23, which now a use which requires approval by special exception or does not meet the established separating distance, shall be allowed to expand up to 15% of the existing square footage by site plan approval provided the expansion does not bring the on-site activity any closer to the residential zone.

If an applicant/developer cannot identify the specific application for a project (e.g. is it a warehouse or is it a truck terminal) the PZC will assume the highest level of intensity and apply the most stringent requirements. The project will be subject to a special exception at a minimum. Changes in the use after the original Commission approval may require additional approvals from this Commission.

7.24.2 Provisions

- A. At the time of application, all new facilities shall comply with the separating distances outlined above. A radius map illustrating the separating distance shall be provided. An anticipated truck route shall be provided.
- B. Details of the hours of operation for activities is required. The Commission may limit activities to specific hours to reduce impacts on residences.
- C. Signage for directional guidance for vehicles entering and exiting the facility shall be provided on-site, including directional guidance to the nearest truck route minimizing truck traffic in residential areas.

7.24.2 Buffers/Screening

- A. Buffer width shall in accordance with Section 6.2.4.C Buffer Widths, and shall be designed to conform to the requirements of Section 6.2.4.
- B. Trucking operations in a commercial zone may be required to provide screening around all sides of the development. See Section 6.2.5 Screening requirements.

7.24.3 Additional Application Information

The following additional information shall be provided at the time of application:

- 1. A general floor plan which illustrates the layout of the proposed uses;
- 2. A narrative detailing such items as: number of docks doors, hours of operation, proposed uses, level of “intensity” – i.e. the frequency at which products/materials are moved in and out of a facility; and
- 3. Any operation and outside maintenance plan for the facility including snow clearing operations, maintenance of detention basins, pavement markings, signage etc. shall be shown on the plans.

Add to Section 10.2 Definitions

Access drive - a private driveway connecting a street with a parking or loading area with sufficient width to permit the safe passage of all vehicles, equipment, machinery, trailers, manufactured homes, boats and/or pedestrians, either self- propelled or transported, which may normally or reasonably be expected to seek access to the parking or loading area or space.

Section 8.4 Special Exception Standards and Procedures

A. Application Submission Requirements

Applications for a Special Exception shall include the following:

1. Site Development Plan application as required in Section 8.6 Standards for Maps and Plans; and
2. A narrative statement describing the proposed use.

B. Review Criteria

The Commission may, in appropriate cases and subject to appropriate modifications and safeguards prescribed by it, grant a Special Exception/Site Development Plan approval subject to the requirements of these regulations. Applications for a Special Exception may be approved if the Commission determines that:

1. The proposal is consistent with the goals and objectives of the Plan of Conservation and Development.
2. The application has met the requirements of the zoning regulations.
3. The land is physically suited to the proposed use.
4. Minimal, if any, adverse environmental impacts are created **including emissions**.
5. No traffic or other hazards will be created.
6. The impacts on the capacity of the present and proposed utilities, street, drainage systems, sidewalks, and other elements of the infrastructure will be minimal.
7. There will be minimal or no adverse effects on existing uses in the area.
8. Surrounding property values will be conserved.
9. The character of the neighborhood will be maintained or minimally disrupted.
10. The general welfare of the community will be served.
11. There is a balance between neighborhood acceptance and community needs.
12. Historic factors are adequately protected; or due consideration to preservation of historic factors has been demonstrated.
13. The overall physical appearance of the proposed development is compatible with surrounding development and the Commission's goals for the neighborhood/corridor.
14. The architectural design is aesthetically pleasing and blends well into the surrounding area.
15. **Noise created by on site activities, including vehicle equipment noise.**

The Commission may impose additional conditions in accordance with these regulations in order to ensure that all applicable criteria enumerated above and/or within a particular use category are satisfied.

C. Changes

Any substantial changes in facilities or uses to that approved in the Special Exception application shall require additional approval of the Commission, and, at the discretion of the Commission, may require a new application. The exception is when a Common Interest Community allows minor construction such as a deck within the previously approved envelope only.

Sec 10.2: Add Definition:

Screening - a visual barrier between an unsightly or out of scale feature or incompatible land uses or activities and the view from public streets and abutting properties.

Add Section 11.9 Appendix I Buffer and Landscaping Designs

11.9.1 Buffer design criteria to be incorporated to address mitigation of environmental conditions listed below. If the applicant is requesting a reduction of the buffer, the applicant shall demonstrate the benefits of the designed buffer. The incorporation of a wall into the buffer is desirable with reduced buffers on high intensity uses. A statement from the landscape architect shall be included that addresses how the design criteria have been incorporated into the proposed plan.

A. Air quality - Buffers can affect the surrounding environment by temperature reduction, air pollutant removal and energy effects on building. The following are suggested design principles:

1. Design considerations should consider topographical, meteorological and other landscape scale factors; plant buffers close to the air pollution sources(s);
2. Plant moderately dense buffers for best air pollution removal; use trees, shrubs and grasses for multi-layered trapping; select plants with dense branching and twig structure;
3. Plant buffers in energy conserving locations managing the landscaping for shade and wind;
4. Plant trees that have leaves with hairy, resinous and coarse surfaces to capture more particles than smooth leaves;
5. Use multiple species to minimize risks with low diversity;
6. Use long-lived species that requires minimal maintenance; and/or
7. Select species with disease and pest resistance and are suitable for the site.

B. Noise Control - Buffers can reduce noise from roads, driveways and other on-site sources. The following are suggested design principles;

1. Along roadways, locate the plantings close to the noise source while providing an appropriate setback for pedestrian access and snow removal;
2. Choose evergreen species best at providing year-round noise control;
3. Create a dense buffer with trees and shrubs to prevent gaps in the landscaping;
4. Select plants that are tolerant of air pollution and de-icing methods;
5. Consider topography and using existing landforms as noise barriers where possible; and/or
6. Construct noise barrier walls.

C. Visual Screening – Buffers can provide visual screening of activities on adjacent sites. The following are suggested design principles:

1. Create a design with dense and multi-layered vegetation, particularly shrubs to screen views; and
2. Plant tree species that have maximum screening values. Deciduous plants provide 40% less screening than evergreens after leaf fall, so evergreens or a wider deciduous buffer may be necessary for screening year-round. Consider vegetation and viewpoint height in design of the buffer.

D. Crop Pollinator Habitat – Landscaping plans can provide valuable resources for crop pollinators including shade, nesting sites, water, nectar, pollen and protection from pesticides. Buffers can be designed to reduce wind and aid in foraging and pollination efficiency.

11.9.2 Buffer Cross Sections

Cross Section Samples attached – examples will be incorporated into pages of the final zoning book

Add Section 11.9 Appendix J Types of Warehouses

Type of warehouses

Warehouse - Can be referred as a high cube warehouse; typically, a building at least 200,000 gross square feet with ceiling height of 24+ feet. Used for storage/consolidation of goods prior to their distribution. Typically has a high level of automation and high-efficiency processing enabling inside workers to process orders that leave the building. Medium level of intensity as goods are often housed for more than a month resulting in fewer issues with traffic congestion and queuing. Loading dock typically on one side. Usually lower number of dock doors – one per 20,000 sq. ft. Will have limited hours of operation – generally M-F early morning to late afternoon. Fewer employees and parking spaces versus other facilities due to lower intensity. Noise and air pollution should not be a factor. Warehouses over 40,000 sf allowed by special exception. Distance from a residential zone - a minimum 500 feet.

Distribution Center - Many of the same elements as a Warehouse as the two terms are often used interchangeably or together. Distribution centers will have a higher level of intensity with goods moving in and out daily. Increased intensity results in greater traffic/queuing issues. Typically has more dock doors – i.e. one per 10,000 to 20,000 sq. ft. Also may have dock doors on two adjacent sides. Higher levels of automation versus Warehouse and greater truck parking needs. Greater noise and air pollution as a result of increased truck activity. Requires more employees and parking spaces per capita than Warehouse as a result of greater activity and intensity. May require 24/7 operation. Distance from residential zone- a minimum 500 feet.

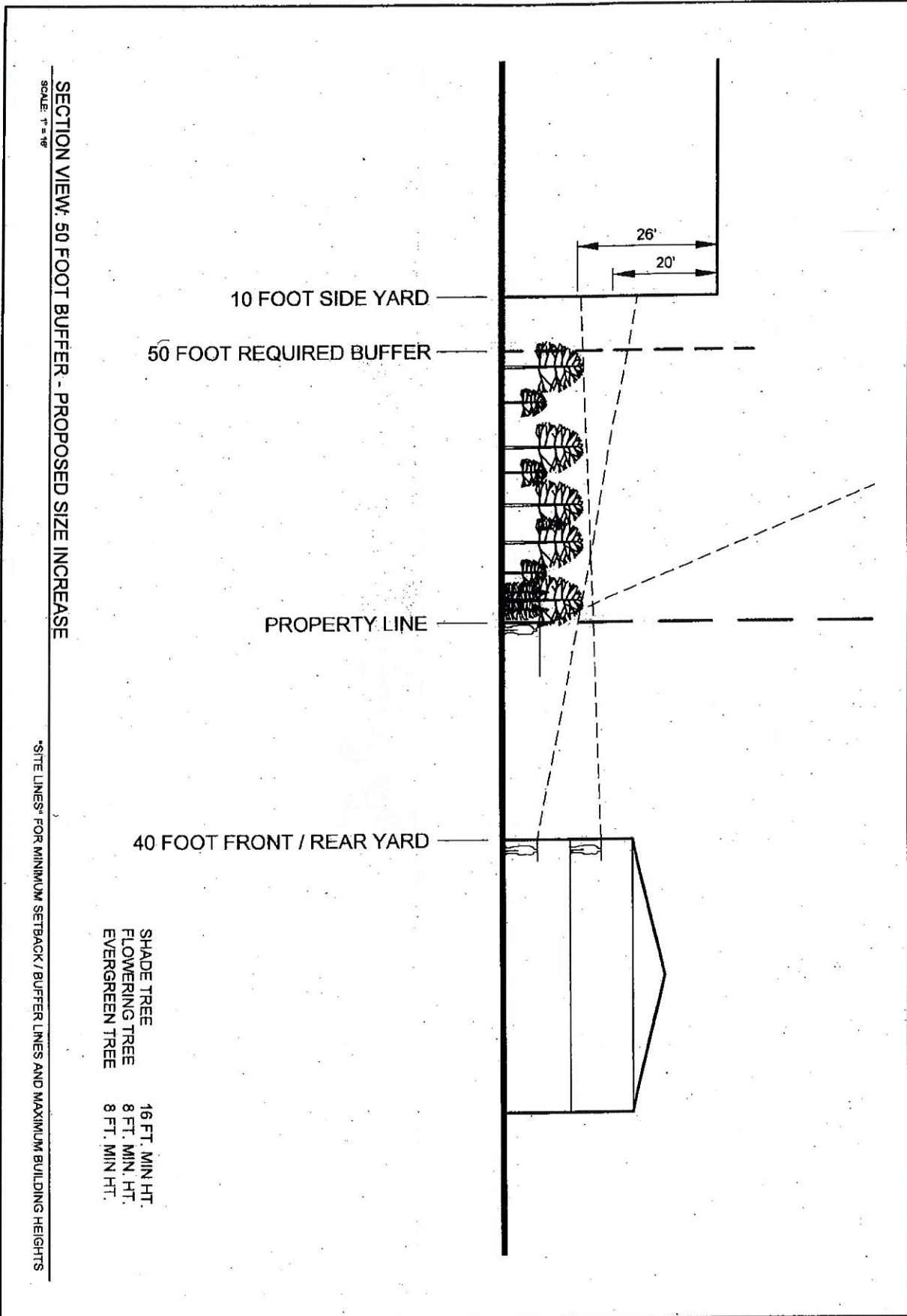
Bus/truck storage facility - Also referred to as a bus yard or bus depot. This is any area where buses – or other large commercial or industrial vehicles - are housed and maintained. Generally, this facility would be high intensity as loud and heavy vehicles are entering and exiting frequently throughout the day. This facility would generate high levels of pollution, noise and other forms of disruptions as a result of the inherent activities at the facility (i.e. Maintenance, repairs, cleaning, etc.). The structures on the lot might vary per the needs of the use, but would generally include large garage and bay space, and office space. Much of the activity occurs on the grounds, not in the building. While not likely 24/7 operation, it could be. Distance from residential zones -a-minimum of 750 feet

Freight Terminal - Also called a Transload Facility or a Parcel Hub or cross-dock facility. High intensity with high number of dock doors – typically 1 per 10,000 sq. ft. but can range to 1 per 5,000 – 15,000 sq. ft. May require dock doors on 2 or more sides. Typically has little storage as main purpose is for rapid transfer of loads from one vehicle to another. Most industrial of all applications with concentration of traffic moving in and out hourly. Can impact neighborhood traffic and present queuing challenges. Hours of operation often greater than other applications including need for 24/7 operation. Noise and air pollution problematic. Least desirable use near residential areas. May include truck washing, fueling and maintenance areas. Number of employees and parking spaces vary based on operation but generally higher than Warehouse. Distance from residential zones minimum of 1,000 feet.

Last mile delivery facility - Also called a Fulfillment Center. Generally, the highest level of intensity with vehicle traffic in and out throughout the day. Often will use vans or other types of

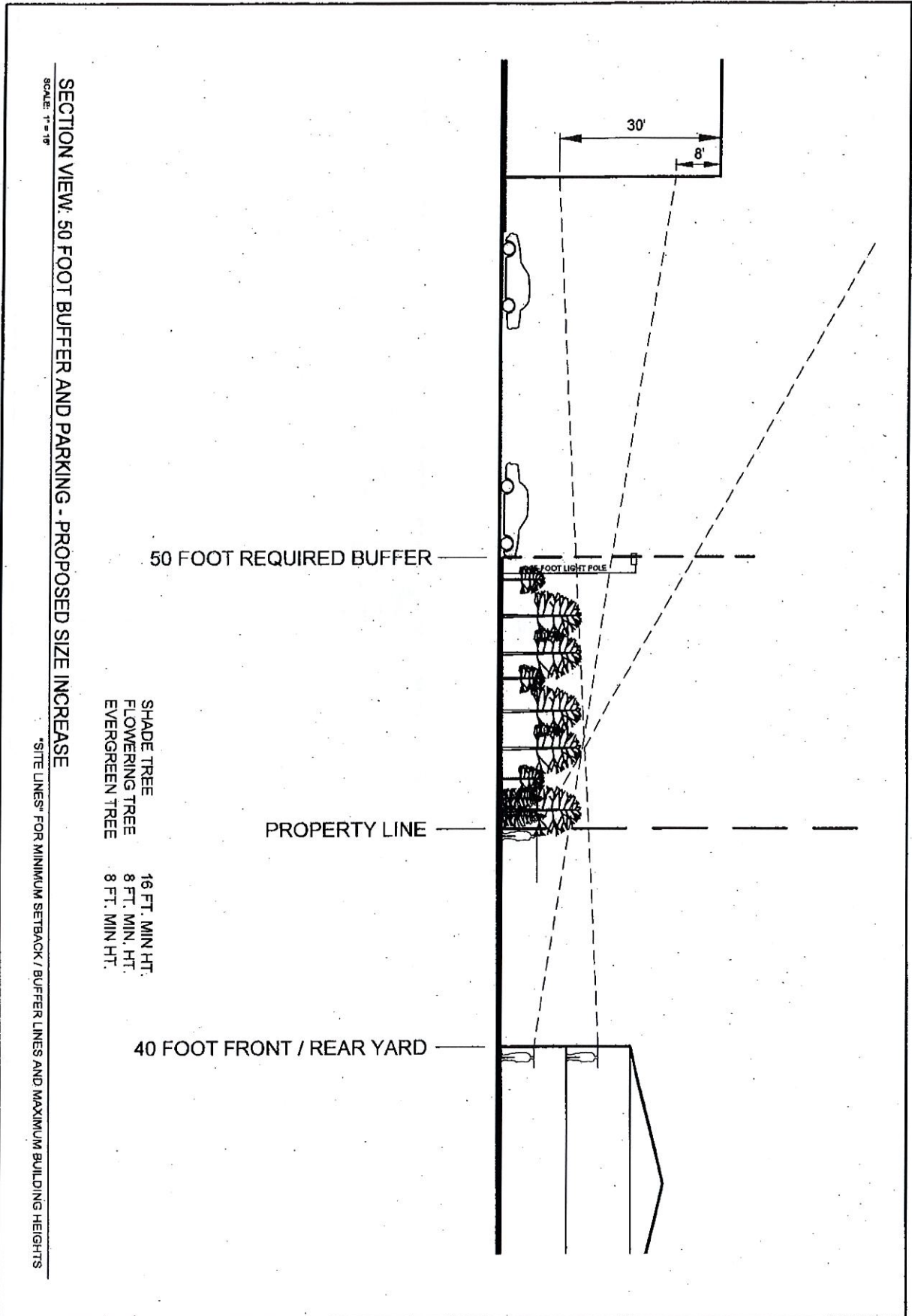
vehicles other than tractor-trailers on out-going distribution, but incoming loads are typically large trucks. Typical facility would be an Amazon Distribution Center. Physical facility similar to Warehouse. Dock doors could be on two sides. Many elements similar to Warehouse or Distribution Center. Intensity requires high – maybe highest – number of employees and parking spots. Noise and air pollution should be moderate as trucks will quickly drop load and leave unlike Distribution Center or Freight Terminal where trucks may linger. Distance from residential zone minimum of 750 feet.

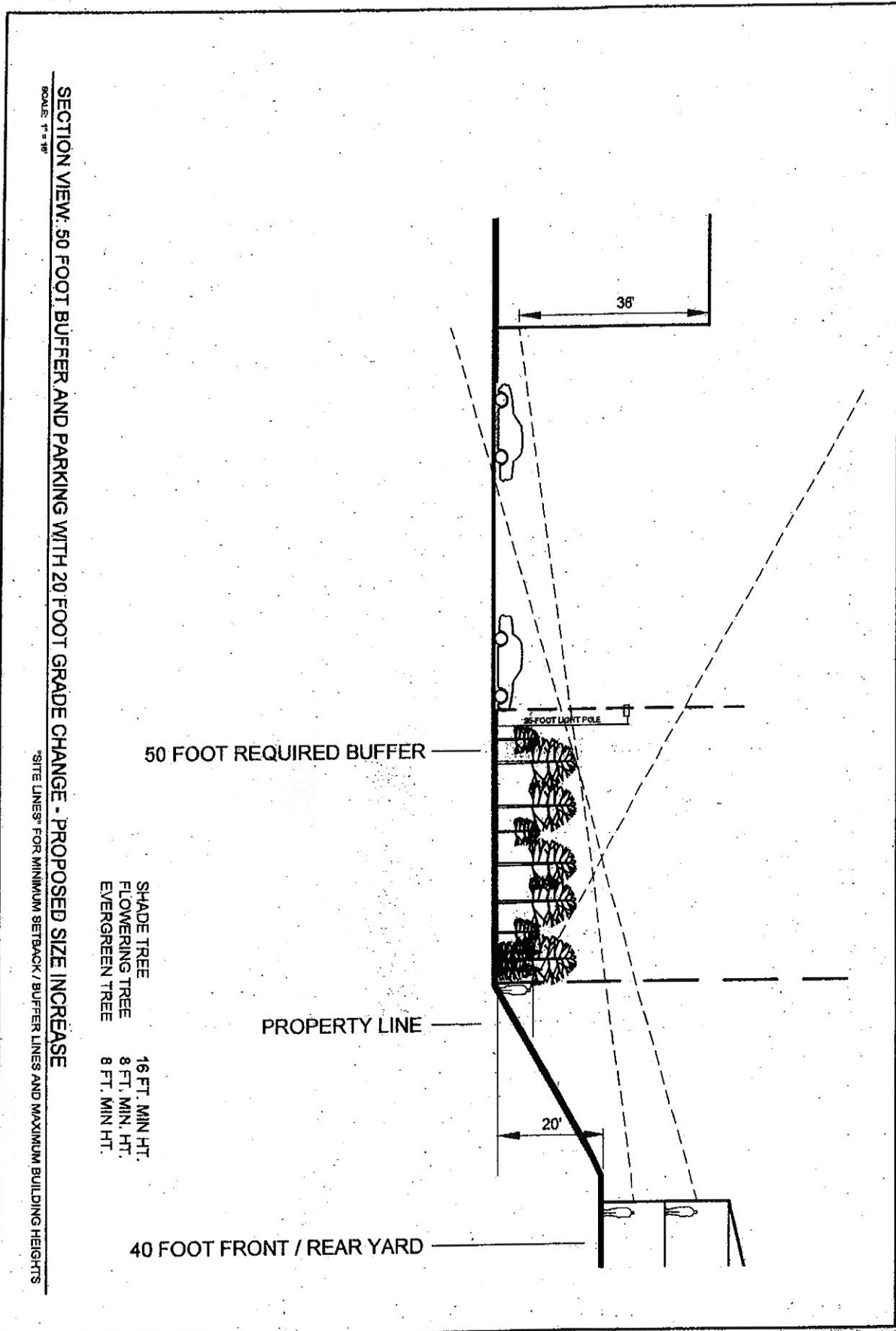
Storage Facility (self- storage) - Lowest level of intensity generally used by individuals or small business to store goods for a long period of time with low level of frequency moving in and out – generally less than once/month. Usually rented out for personal needs like furniture, or even a car. Small cube spaces part of a facility of dozens or even hundreds of units. Not intrusive in terms of traffic, queuing, noise, air pollution. Low impact application. Requires few employees and few parking spaces as functions may self-serve. May require loading dock and/or loading areas on site. Generates little to no noise/air pollution. No standard building configuration; may include climate control features.



SECTION VIEW: 50 FOOT BUFFER - PROPOSED SIZE INCREASE

SITE LINES FOR MINIMUM SETBACK / BUFFER LINES AND MAXIMUM BUILDING HEIGHTS

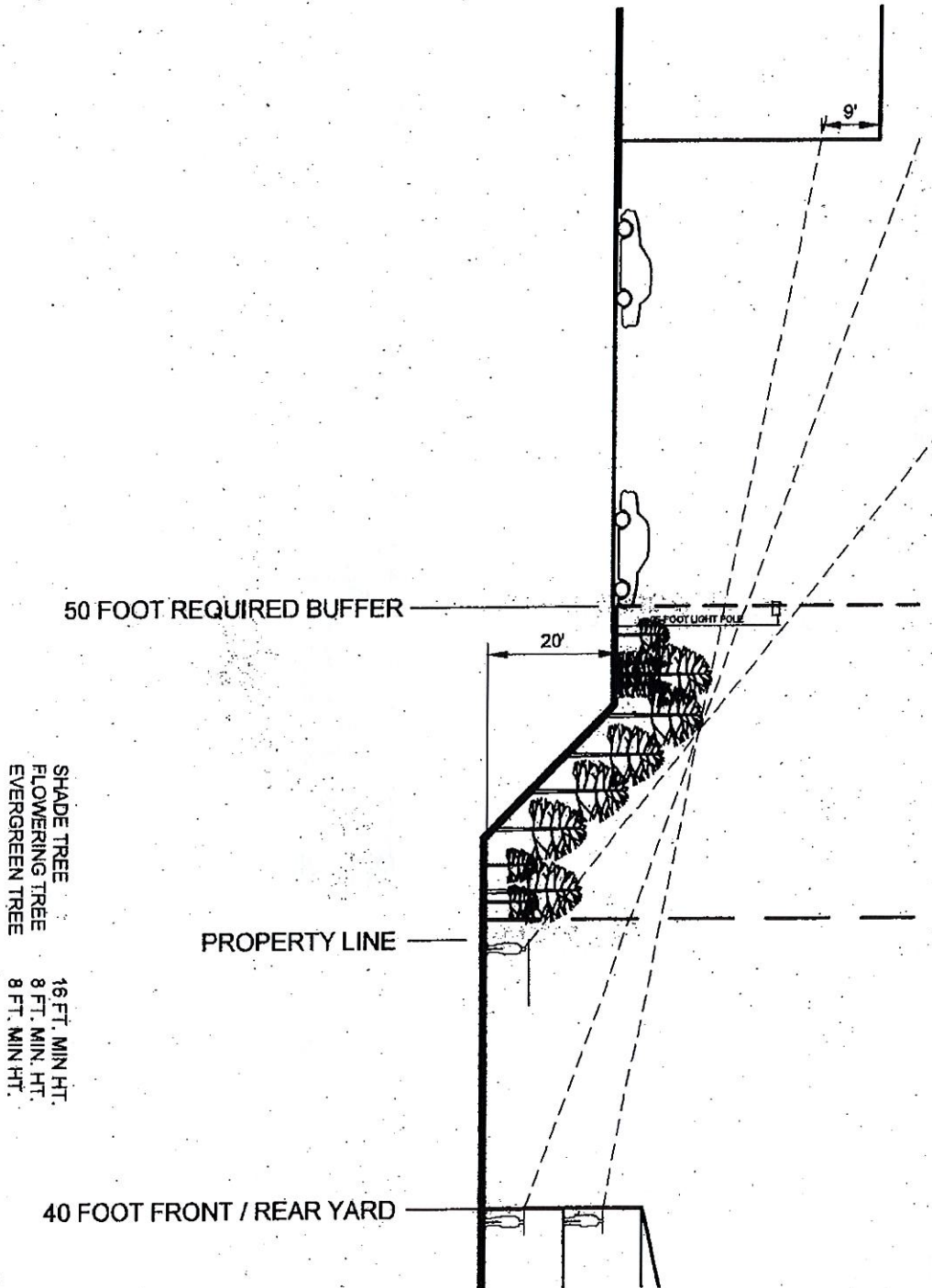


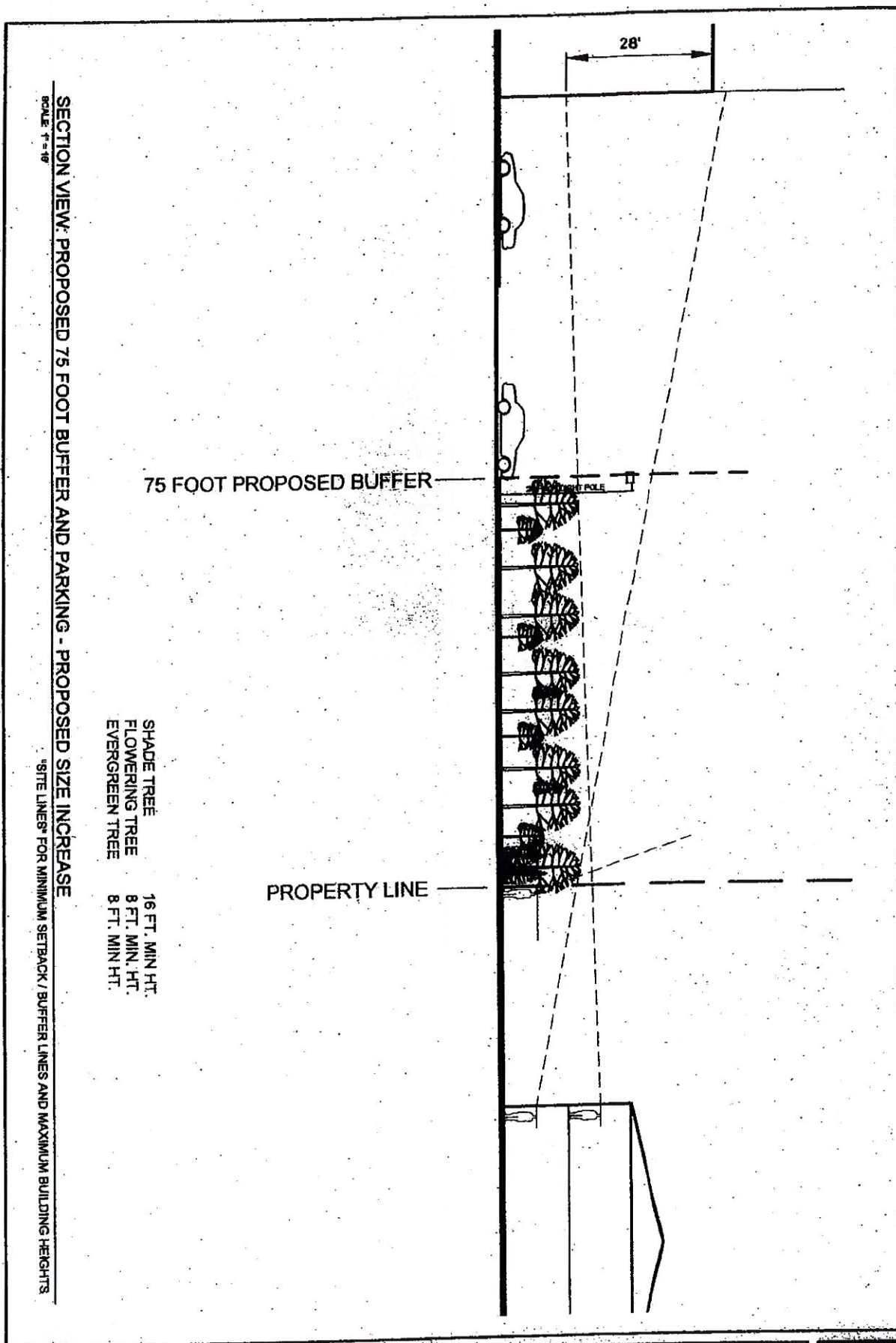


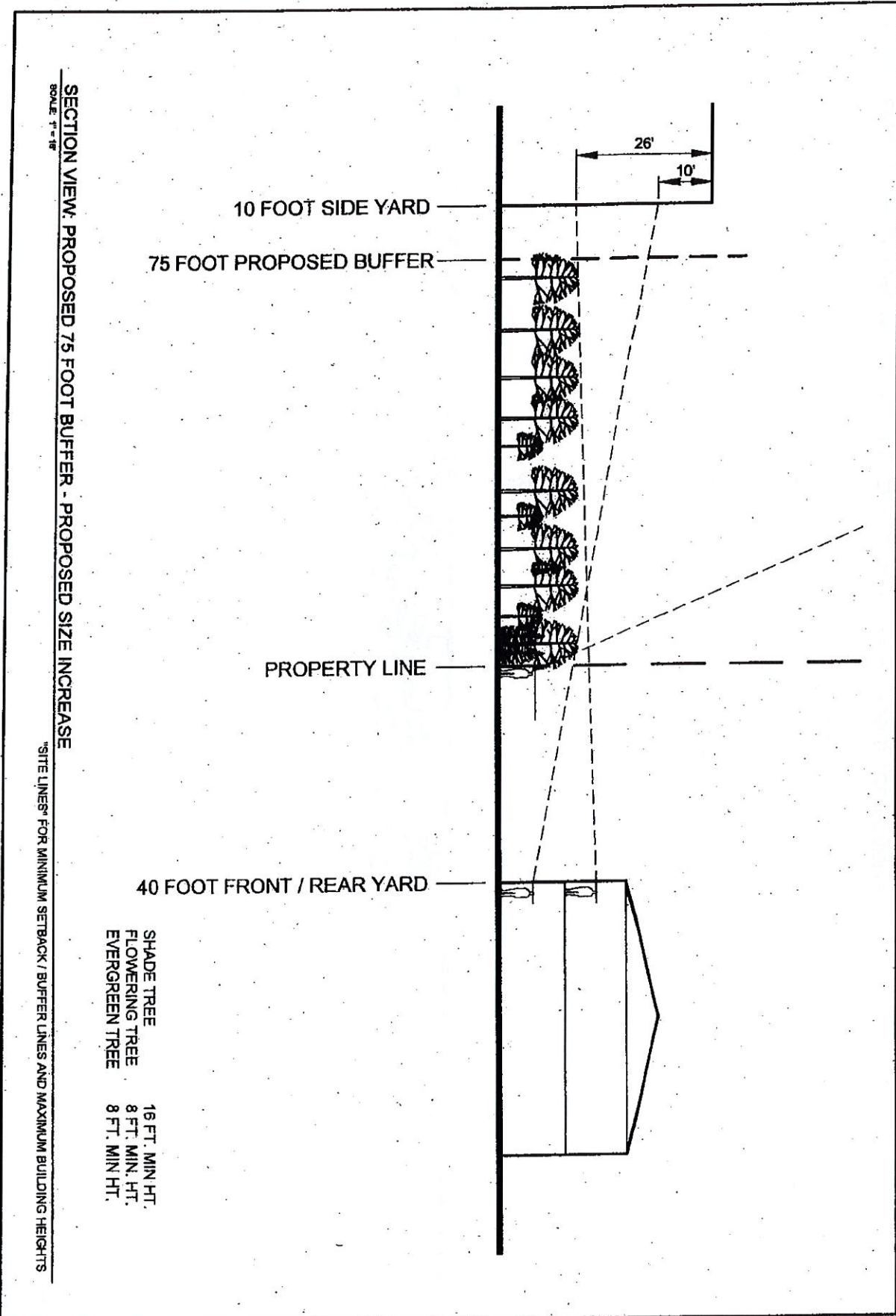
SECTION VIEW: 50 FOOT BUFFER AND PARKING WITH 20 FOOT GRADE CHANGE - PROPOSED SIZE INCREASE

SCALE: 1" = 16'

SITE LINES FOR MINIMUM SETBACK / BUFFER LINES AND MAXIMUM BUILDING HEIGHTS



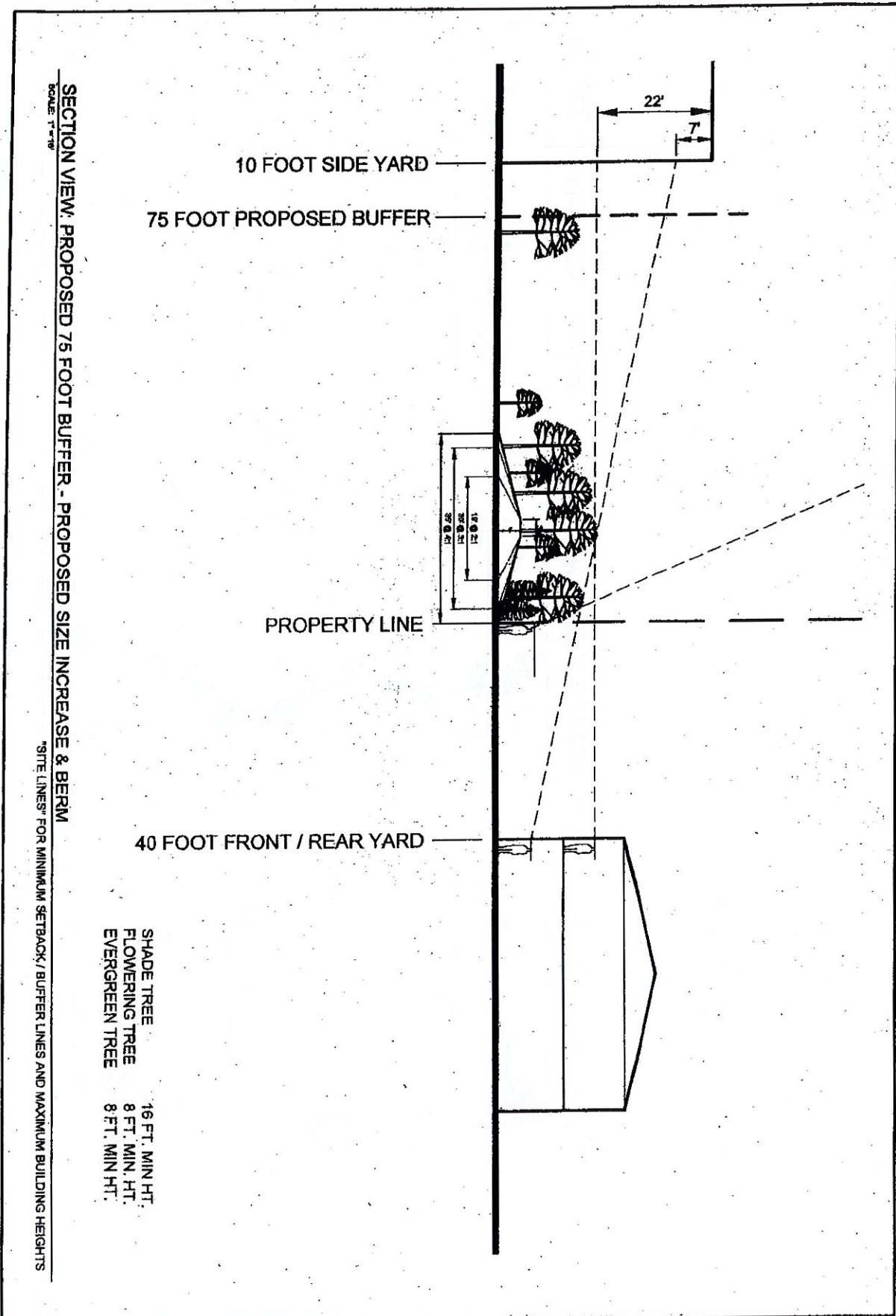


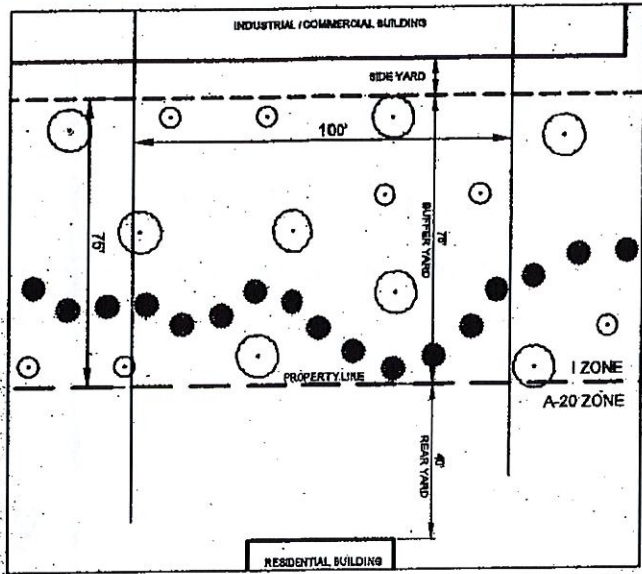


SHADE TREE 16 FT. MIN. HT.
 FLOWERING TREE 8 FT. MIN. HT.
 EVERGREEN TREE 8 FT. MIN. HT.

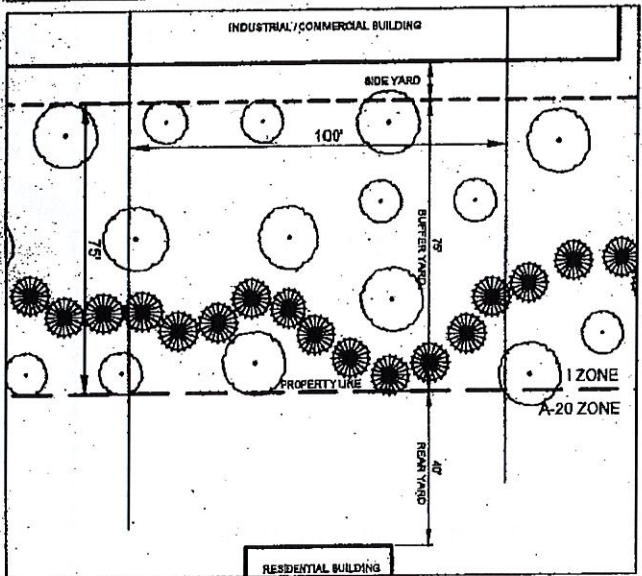
SECTION VIEW: PROPOSED 75 FOOT BUFFER - PROPOSED SIZE INCREASE
 SCALE: 1" = 10'

"SITE LINES" FOR MINIMUM SETBACK / BUFFER LINES AND MAXIMUM BUILDING HEIGHTS

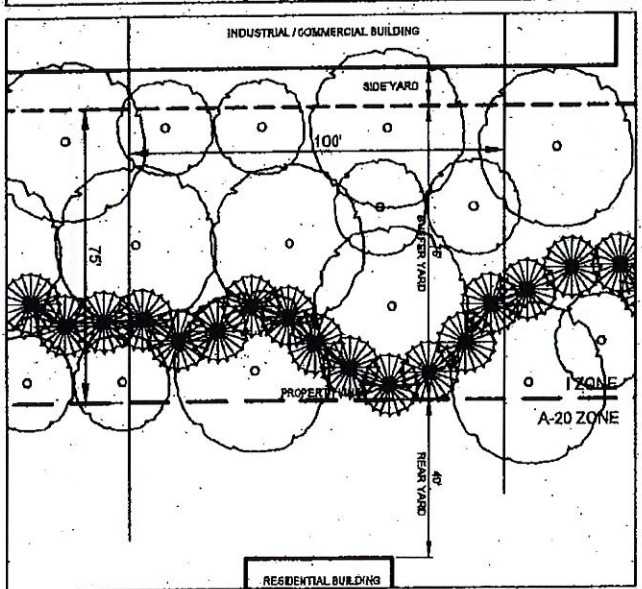




PLAN VIEW: SPACING / INSTALLED SIZES
SCALE: 1" = 20'



PLAN VIEW: SPACING / 5-YEAR SIZES
SCALE: 1" = 20'



PLAN VIEW: SPACING / MATURE SIZES
SCALE: 1" = 20'

PER 100 RUNNING FEET OF 75 FOOT WIDE BUFFER

5 CANOPY TREES	RANGE 4-6
4 MID-SIZE TREES	RANGE 4-6
11 EVERGREEN	RANGE 10-15
CANOPY TREE	16 FT. MIN. HT. 3" CAL.
MID-SIZE TREE	8 FT. MIN. HT. 2" CAL.
EVERGREEN TREE	8 FT. MIN. HT.